

# THE **S** RESOURCE

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## NEWS, REVIEWS, INFORMATION TO USE

*June/July 2021*  
*Volume 7 No. 5*

# SCALE

**New Tracks - Where Mentors Help Modelers Build  
Building the "Nighthawks" Scene  
The Minnesota Heartland  
S Scale Schnabel Car Shows, Meets and so much more**



# O&S Scale Midwest Show



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Dwight, Illinois 60420  
815-584-1577

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**June/July 2021**  
Volume 7 No. 5

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*Owner / Publisher*  
**Amy Dawdy**

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*Managing Editor*  
**Daniel Dawdy**

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*Advertising Manager*  
**Jeb Kriigel**

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Welcome to the online S Scale Resource magazine. The magazine is presented in an easy to use format. The blue bar above the magazine has commands for previewing all the pages, advancing the pages forward or back, searching to go to a specific page, enlarging pages, printing pages, enlarging the view to full screen, and downloading a copy to your computer.

### Front Cover Photo

*Looking down on the beautifully built  
“Nighthawks” scene. Built and photographed by  
Chris Monje.*

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The Model Railroad Resource, LLC publishes *The O Scale Resource* and *The S Scale Resource*. Be sure to look at both of our magazines. There are many articles in our magazines that are not scale specific and will be of interest to you. Click the magazine title in this announcement to see the magazine.



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## NEW COLORS

### JUNE, 2021

- TCP-386 Toronto, Hamilton & Buffalo- Maroon
- TCP-389 Elgin, Joliet & Eastern- Orange
- TCP-393 ONE Container- Pink

### JULY, 2021

- TCP-390 Canadian Pacific- Modern Freight Car Brown
- TCP-391 Amtrak- Phase V Blue



Tru-Color Paint is a solvent-based paint, which uses an acrylic polymer to bind our finely-ground pigments to the model being painted. Tru-Color paints are intended to be used by airbrush, right out of the bottle, at 28-35 PSI and a medium tip. Clean up of the air-brush equipment or paint brush is easily accomplished with acetone. If thinning is desired of any paint, we recommend using our TCP-015: Thinner, at 5%-15% (maximum) concentration. For hot (>90° F) or humid areas, we recommend adding TCP-310: Retarder to the paint to slowdown drying time. All of our paints are available in standard 1-ounce & 2-ounce bottles. 16-ounce (and greater) quantities are available- just call or email for prices. See our website for a complete list of our paints, or email us for a brochure for any of the categories of paints we manufacture.

## FEATURED RELEASES

- TCP-013 Aluminum
- TCP-077 Silver
- TCP-085 Gold
- TCP-287 Copper
- TCP-288 Brass
- TCP-354 Galvanized Steel
- TCP-355 Dark Aluminum
- TCP-357 Burnt Iron
- TCP-713 Metallic Dark Pewter
- TCP-717 Metallic Light Pewter
- TCP-758 Magnetic (Dark Gray Metallic)
- TCP-774 Metallic Tin
- TCP-1226 Titanium



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## The Pacifics Have Arrived!

*These beauties were built in 15 different versions across the Southern Pacific P-10's, Chesapeake & Ohio F-17's and F-19's. The Master Craftsman at Boo Rim Precision did themselves proud with this project! There are still a few models available. Contact us to see availability or check out our website.*



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# From the Publisher's Desk



Summer is upon us here in the US, and that means outside stuff gets in the way of modeling. Well, sometimes time management does work. The good news is we are getting back to normal here and shows are starting up once again.

**Our O&S Scale Midwest has changed dates!** Our contract with the hotel was in place, but another group, a very large group, wanted to use our Friday. So we were in the driver's seat for once. The new dates are October 8-10, 2021. The show admission will stay at \$20 for pre-registration prior to the show. Admission at the door will be \$25.00. The hotel also lowered the room rate to \$105 per night – a fantastic rate considering the location. So, they helped us and we helped them, and the show will go on! Make you reservation for this show and, of course, help us make our room night requirement by staying at the convention hotel. Like it or not, all of us putting on these large shows are dependent on room nights to keep the costs down. Trust me, no one is getting rich putting on train shows, and breaking even is sometimes the result.

- **Our Website is:** <https://oscalemidwest.com/>
- **You can Register online here:** <https://ribbonrail.com/IndyShows/index.php>
- **Or you can download and print out the form here:** <https://oscalemidwest.com/printable-show-registration/>
- **And then, you may register at the hotel here:** <https://oscalemidwest.com/on-line-hotel-registration/>

Ken Zieska and Bill Lane have a weekly Zoom meetup on Tuesday nights. It is called an “S Scale Drop In”, and everyone is welcome. It's every Tuesday at 8pm Eastern Standard US time; hosted by Ken Zieska or Bill Lane. The link is <https://us02web.zoom.us/j/83595908232> – the Meeting ID: 835 9590 8232 and Passcode: 356426 will be needed. Reminders are posted in the [S Fine Scale Model Railroading Facebook group](#) .

We are really pushing for people to send us articles and pictures of what you are modeling. I know there are some great things going on as I see them on Facebook and some other groups. To help get the word out about what you are doing, and to showcase your work to many more people, as well as, getting into more detail on how you did what you did, let us know.

This month we revisit the Ken Zieska's Minnesota Heartland. It was six years ago that Amy and I visited it, and a lot has changed as you will see in Ken's article. Chris Monje has a great build of [Wit & Wisdom Models's "Nighthawks" kit](#). I saw the O scale version last October and it's magnificent. Chris built the whole scene, but you can also buy the cafe only.

That's about all from here. Again, please let us know what you are up to, and have a safe summer!

Happy Reading & Happy Modeling,

*Dan Dawdy*

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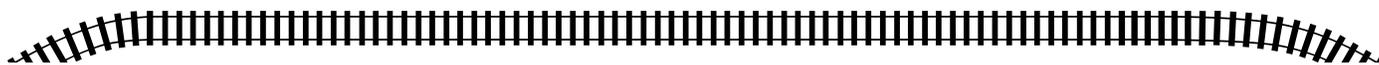
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# THE O RESOURCE NEWS, REVIEWS, INFORMATION TO USE ONLINE MAGAZINE SCALE

**Don't forget to  
read our other  
magazine, The O  
Scale Resource, for  
more modeling  
ideas.**

# NEWS YOU CAN USE



[Berkshire Valley Models](#) is a staple in O scale detail parts and now offers some in S as well. Some of the O scale parts will also work in S. New product is their #672 Motors. These are approximately 5 HP in O scale, 6 HP in S scale, and 10 HP in HO and are white metal.



[See their Website for more details.](#)

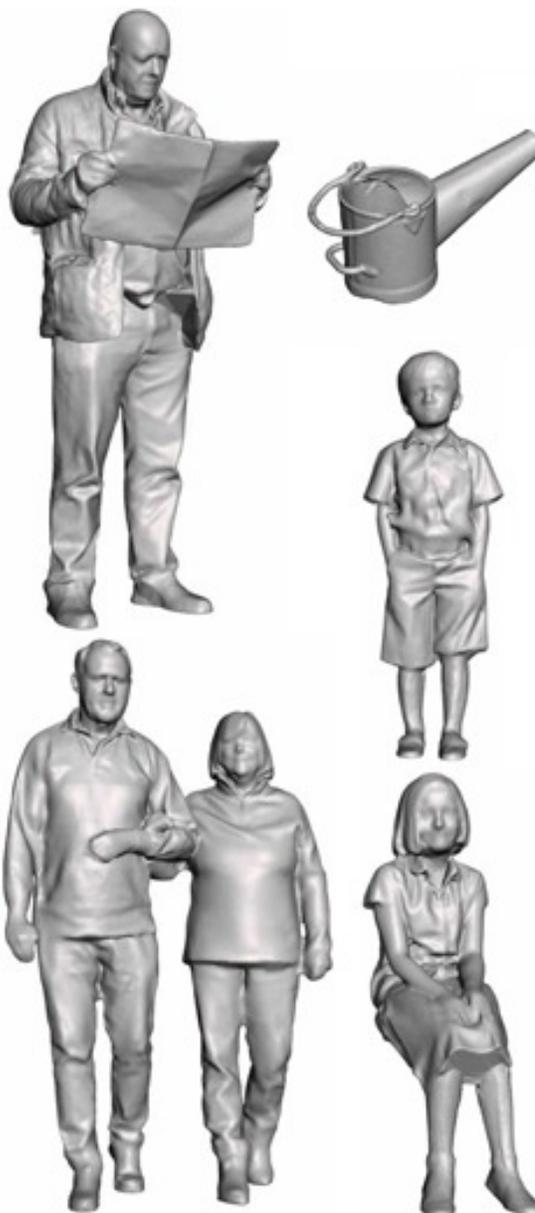


[Model Tech Studios LLC](#) has a some cardboard box piles, over 24 boxes. Who doesn't need aged old, used piles of cardboard boxes? They come pre-assembled and finished. When you see cardboard boxes you don't just see 1 or 2, you see dozens. This pack has over 24 boxes in 4 piles. Use individually or abut to make larger linear stacks.



[Check their Website for all their fine products.](#)

[Modelu](#) is making some fantastic 3D scanned and printed figures and detail parts. Most figures can be ordered in S scale or any other scale. Now, a lot of these are slanted for British railways, but many will work in an American setting. I quickly went through pages of their site and found these.



[Check them out on their Website today.](#)

Tru-Color Paint continues to bring new colors to their fantastic line up. Below are the new product information for the railroad and brushables paints and aerosols for June-July, 2021.

June, 2021 Railroad

- 386- Toronto, Hamilton & Buffalo- Maroon
- 389- Elgin, Joliet & Eastern- Orange

Aerosols

- 4024- Antifouling Red
- 4025- Burnt Iron

July, 2021 Railroad

- 390- Canadian Pacific- Modern Freight Car Brown
- 391- Amtrak- Phase V Blue

Aerosols

- 4026- Grime
- 4027- Matte Mud

As always, Tru-Color Paint is open to new ideas for paints. If there are colors that our readers need but aren't made, have them shoot an email to us at [tru.colorpaint1@yahoo.com](mailto:tru.colorpaint1@yahoo.com). If they can find enough information on the color, it could be put in the next year's production schedule. See their full line up at their Website!



Code 83 switch components are now available from S Scale Track Works. These products were developed as a result of customer request. S Scale Track Works will consider requests like this with an initial order of 10 parts and a deposit. The patterns will be produced and the initial order filled. After that, the products will become part of the product line. The initial order need not come from one person. If you have friends that would like the same things you would, group the initial order. This policy helps us cover the cost of the patterns, and shows us that there is real interest in the products.

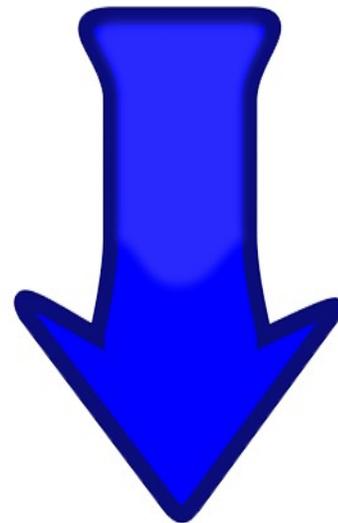
The new Code 83 products include a #8 bolted frog, a #10 bolted frog, guard rails, and switch points. These will compliment our already existing code 83 #6 bolted frog. The parts are at the foundry in the USA and being cast in Nickel Silver. The parts will arrive at our facilities during the first week of June. See our website for availability.



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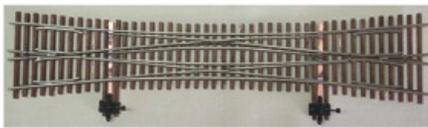
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# Building the “Nighthawks” Scene

By Chris Monje



This model is based on Edward Hopper’s 1943 painting “Nighthawks”. John Armstrong designed the scene, and described his thought process in the January 1989 issue of *Model Railroader* magazine. Over the years, John wrote several *Model Railroader* articles about his Canandaigua Southern O gauge railroad.

An excellent kit based on John’s work is marketed by [Wit and Wisdom Models](#), and exclusively built by [Right On Track Models](#) for [Wit and Wisdom Models](#).

This is an S scale kit, three structures in the scene are described here: 1) the one story diner attached to a two story building, 2) the two story building seen in the painting across the street, and 3) a building flat resembling what John used to the right of the diner.



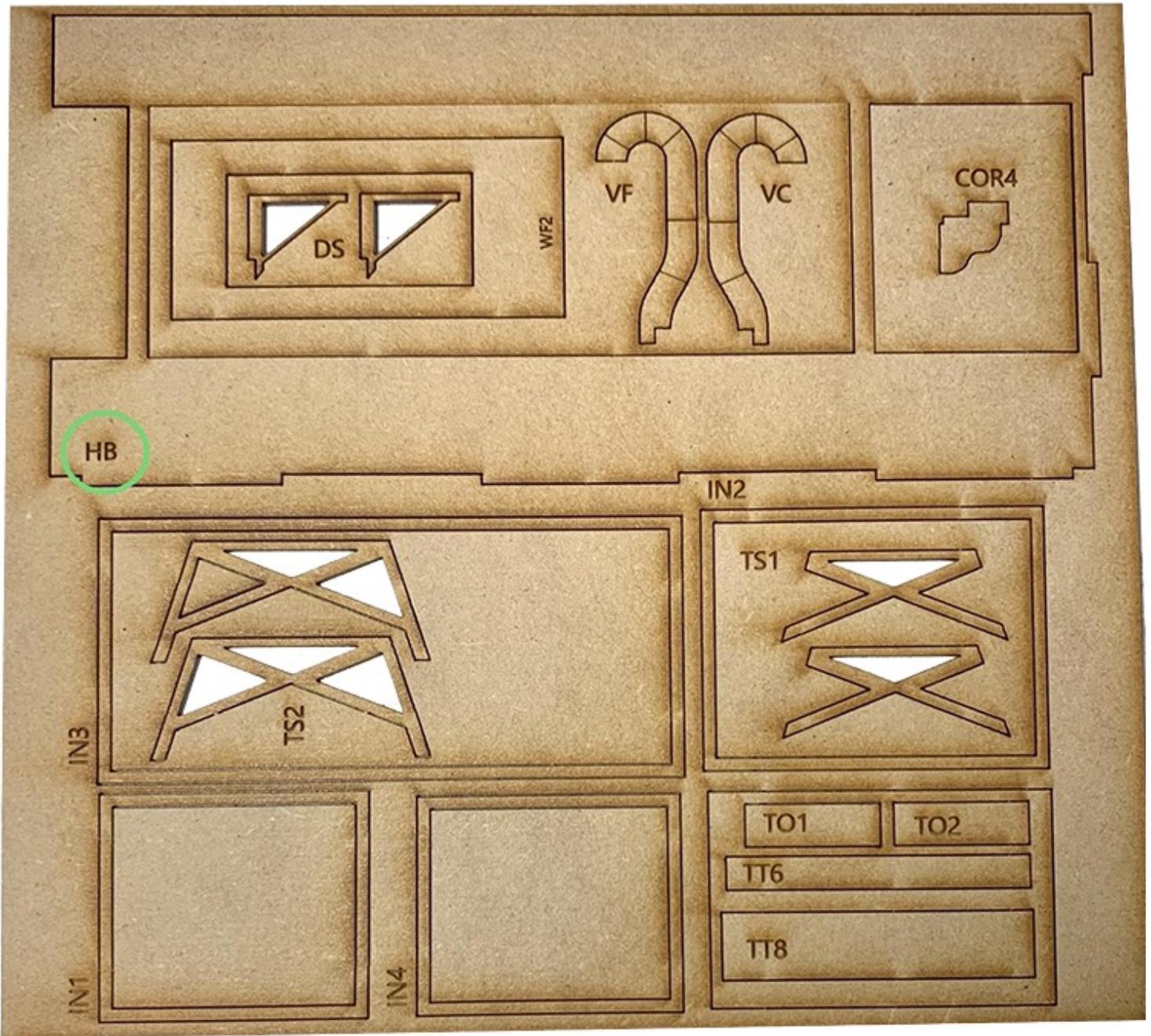
Building this model takes experience, but instructions are good and parts are precise. The full version of the kit includes multiple buildings that replicate the “Nighthawks” painting and more.

Parts are well packed, and there are hundreds of them. The instructions include a photograph of John Armstrong’s model, Edward Hopper’s painting, and the finished Right On Track scene.

The illustrations here chronicle my build of this kit, and are not meant to be specific instructions. The manufacturer supplies 24 pages of good pictures and assembly description, although some of the images are small.

It’s important to familiarize yourself with the instructions and parts before starting assembly. Parts are identified with letters and numbers, but it can be difficult to find them in the early stages of construction. It’s convenient to have a 6 foot table to spread things out. I also kept the shipping box to help organize the parts.

Here's an example of diner window wall frame HB on a parts sheet. Efficient use of material requires that parts be intermingled on the same sheet. Exhaust ducts VF and VC are on this sheet, later I had difficulty finding VA!



I also added lighting, which leads to making roofs removable. This takes additional hours, but I am much enamored of LED lighting! For a good resource on LED lighting see the [January/February 2014 Issue of \*The O Scale Resource Magazine\* here.](#)

### 1) The Diner Building

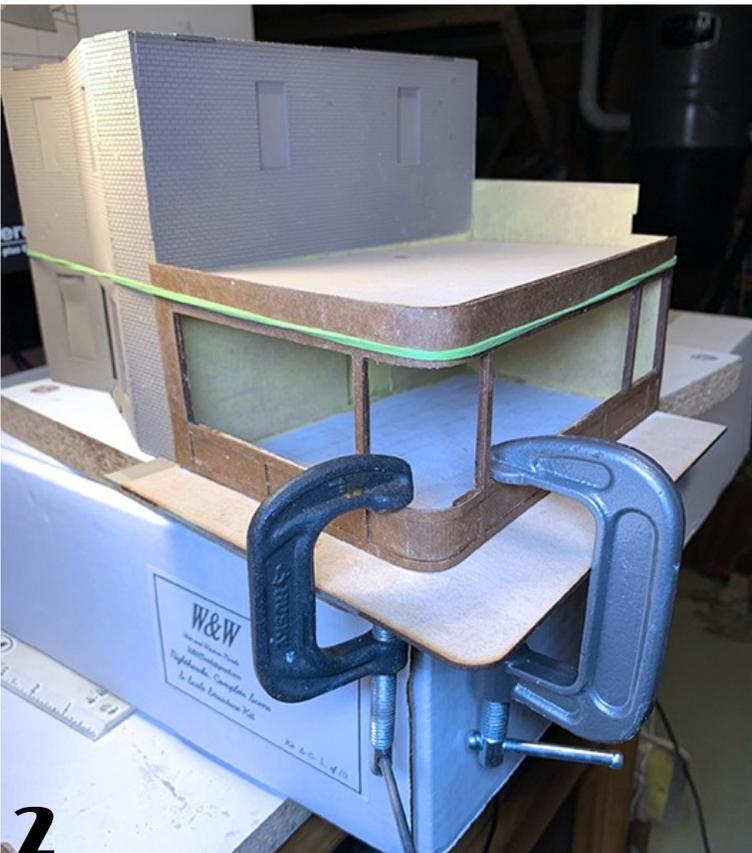
Picture 1, next page, picture shows four parts of the diner exterior. Two sheet material walls, HA and HB, are connected with exterior “skins” SK1 and SK2. In this picture, SK1 has been glued to walls HA and HB. SK2 will next be wrapped around the curved corner and glued to SK1. This is one of the trickier parts of construction. The radius of this corner is about that of a nickel. As seen in this picture, I elected to allow the glue to bond SK1 to SK2 on one side first, then wrap exterior trim SK2 around the corner.



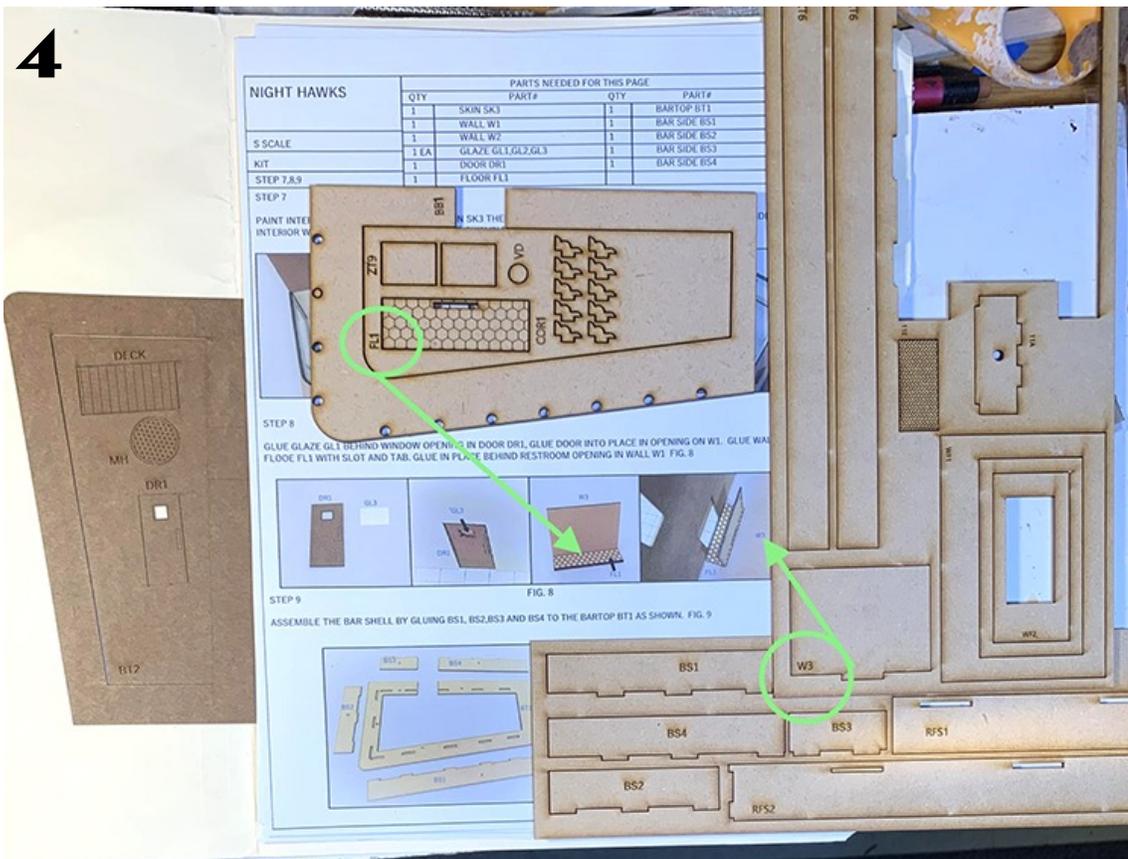
This can lead to misalignment of the window vertical post, but there seems to be some “stretch” in skin SK2.

Incidentally, the sheet material and the “skins” are rather absorbent, so I apply glue liberally to allow it to soak in momentarily. “Squeeze out” can be messy, but I might be avoiding potential delamination later. The green label waterproof Titebond glue is durable.

Picture 2: The rubber band and C clamps illustrate how the roof and building base are used to establish the exterior curve. In this case, the roof is not glued yet, but an unseen piece of scrap copies the roof curve. It’s attached inside the corner with water-activated Gorilla glue.

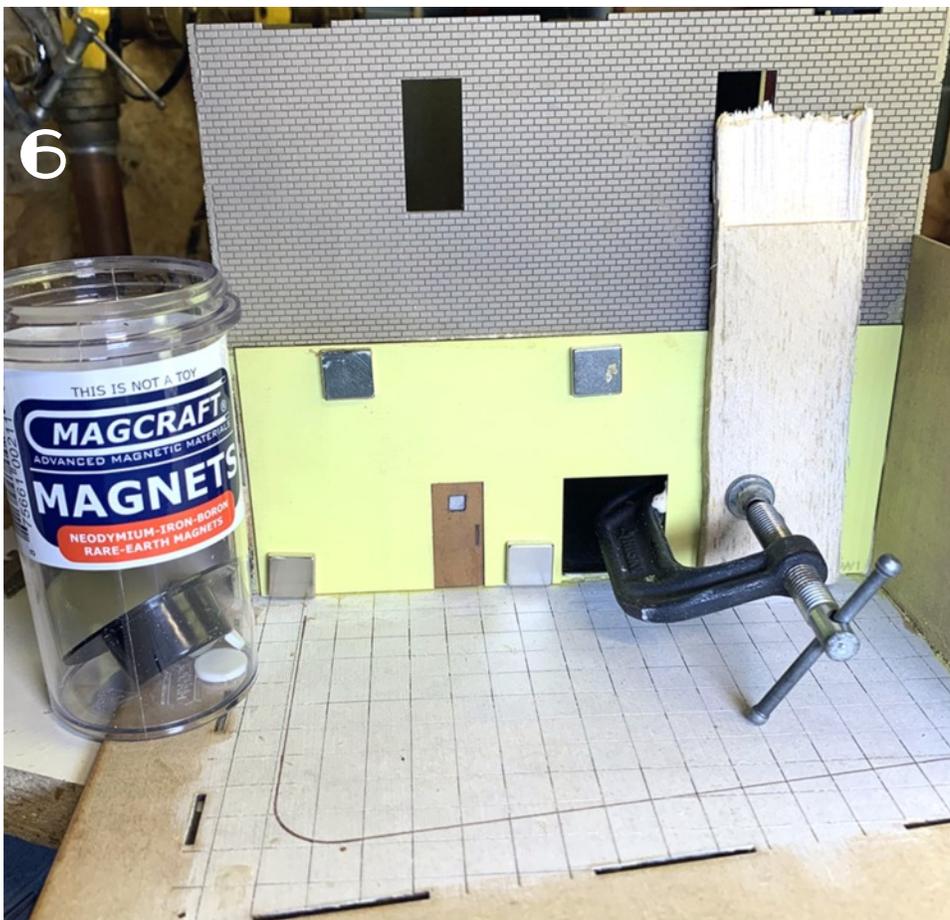
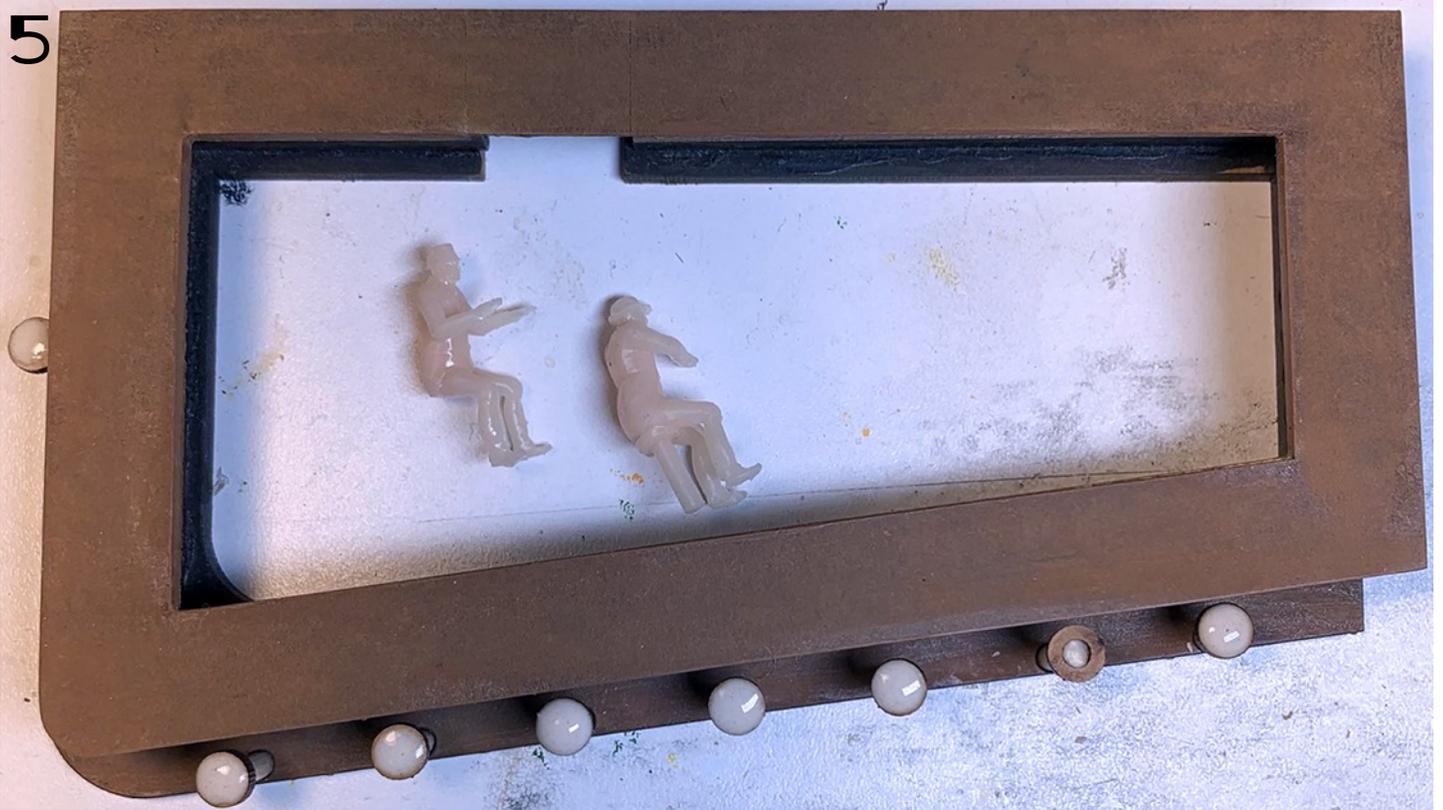


The modified diner ceiling is seen in picture 3. The ceiling is a plastic insert with LED's. It's a removable cutout from roof RF2. The original roof perimeter maintains support for the green exterior facade. Part RF1 seen here is the roof area for the Phillies cigar sign. It goes on top of the cutout for diner roof RF2. The LED's on the ceiling are positioned to be directly above the counter.



Here's another view (picture 4) of how parts are laid out. Within the counter base is FL1, a section of tile floor that goes with wall W3. Wall W3 is a view block in a doorway added by John Armstrong. The doorway is not part of the Edward Hopper painting, but it's an imaginative addition. A photograph of the instructions is included beneath these parts.

“Cushions” for the stools are formed with a puddle of self-leveling glue, followed by a dollop of acrylic burnt sienna. One untreated “stool” shows the dowel and ring. Two of the four figures included with the kit are shown below in picture 5.



This view (picture 6) shows an interior yellow wall secured while glue is drying. Eight strong magnets are used while the C clamp works well for the right side. After the C clamp is removed, the view block wall and tile floor can be installed.

I traced an outline of the counter on the floor, making sure of placement.



This close up view of the painting “Nighthawks” (picture 7 is on a book cover, most images don’t reveal this much detail. I painted the figures accordingly. However, I was chagrined to see an egregious error on my part. The man only has one hand on the counter in the painting. The kit includes two seated men that are reasonably accurate, but I glued them on the wrong stools. However, once the model is in place, this won’t be noticeable. The urban denizens of the night will look more the part once the walls and ceilings are installed.

A chunk of steel holds down the counter that is being glued to the floor.



Details for the counter include cups and salt and pepper shakers. In picture 8, a piece of code 100 rail is on the counter for size comparison.

9



Window installation is one of the last things to do for diner construction. Gluing the curved window is best done by clamping it in place with a  $\frac{5}{8}$ " dowel. Clear Gorilla Glue is good for this job, and water activation is not necessary. It dries clear overnight and is strong enough to hold the curved plastic window. Picture 9.

The cutout roof for the .060" plastic insert is visible in this upside down view. The insert holds the LED lighting.

Picture 10 shows a door alcove being held square while the glue dries. The tab and slot construction keeps things aligned, and the vise and magnets make sure. A bench vise is very handy at times. Note that a hole is laser drilled in the top for a light. This building entrance was designed by John Armstrong for the Nighthawks diner.

10





This picture 11 shows an additional roof I made from .060" black styrene. It's for the two story building that is part of the structure for the one story Nighthawks diner. Roof edging is made from S scale 2x6's.

The roof is a tracing of the building. The angles are part of compression and perspective that John Armstrong designed.

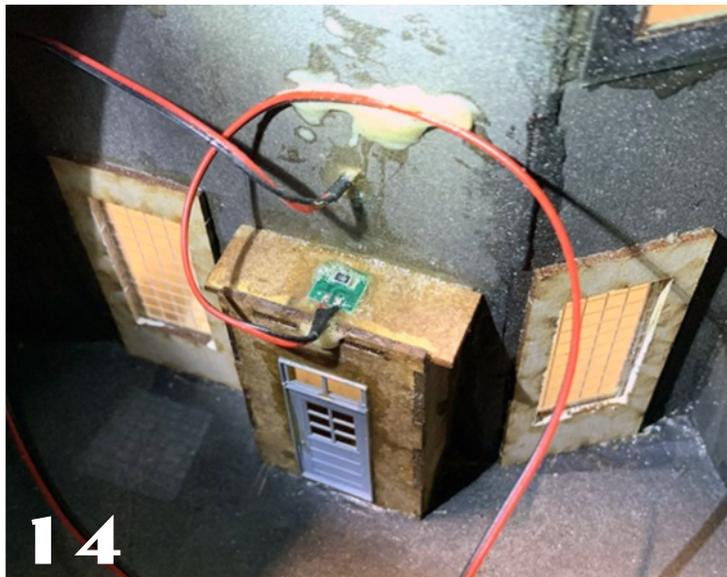
The additional removable roof is for lighting access. The original roof is installed as designed by the manufacturer, but first a 4-1/4" hole saw was used to make a hole. This size saw is typically used for dryer exhaust ducts.

Photo 12 is a picture of the original roof with the access hole. The removable roof is on top along with the hole saw





This street level view 13 shows where the mirror goes. The green circle points out the word “mirror” laser-written on the sheet material. John Armstrong designed this scene to reverse the “CAFE” sign with the mirror image.



I also added two LEDs from Woodland Scenics above this sign and in the doorway. The LED mounting is designed for interior lighting, but I used it outside by drilling a hole for the wire and painting the top of the LED card olive drab. Picture 14.



Here are the two removable roofs that allow access for lighting and detailing. At the top of the picture is the underside of the roof for the two story building. Four pieces of razor blades are visible at four corners, not visible are their corresponding magnets glued underneath the stock roof.

The bottom of the picture,15, shows the ceiling 12 volt LED's illuminated with a 9 volt battery. These are warm white 3 light strips that I think look best at less than 12 volts. They need at least 8 volts.

John Armstrong described how he ran incandescent light bulbs at reduced voltage for longevity.

## 2) The Two Story Building Across The Street

Picture 16 is a view of the partially constructed two story building. Prominent in this photo is a scrap piece being used to reinforce a corner. Part of the exhaust duct that came from this sheet is shown. The duct goes with the diner.

Usually having a roof glued in place would make such reinforcement unnecessary, but this roof will be removable for access if needed.

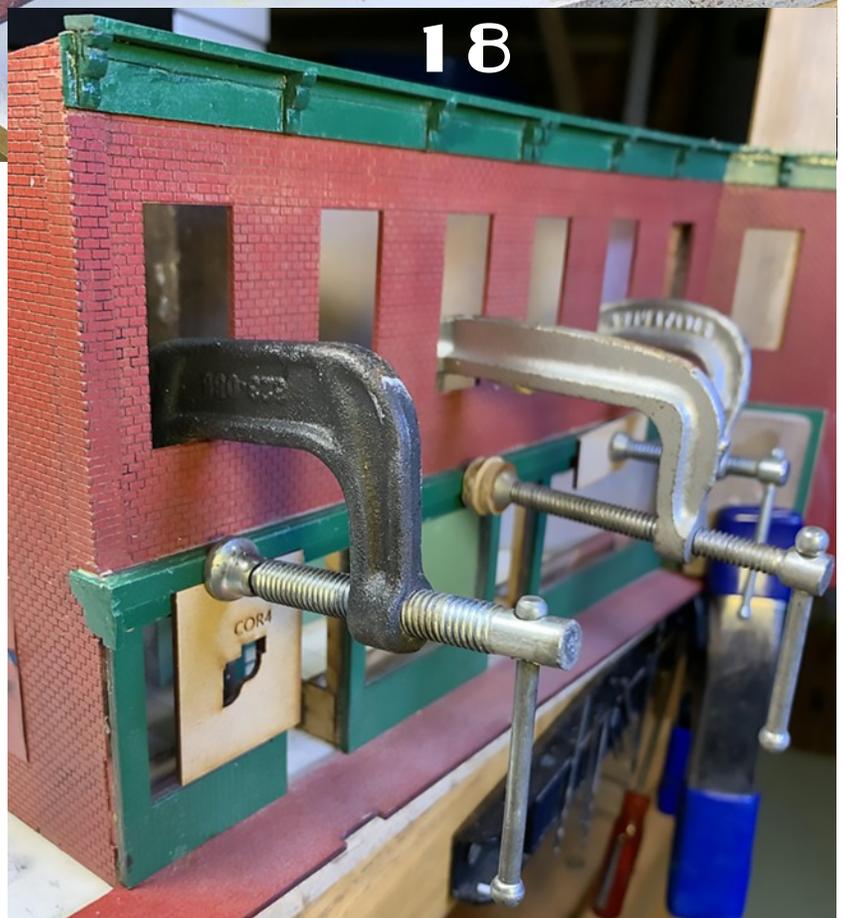
Building trim and interiors are included in the photo. Various components are visible on the parts sheet, two window shelves for the hardware store are painted light brown and one shelf for the barber shop is painted white.

Corner reinforcement isn't necessary with a roof installed, but reinforcement can also be good for stability during handling. This building will have a removable roof for optional access, so reinforcement is more important. The storefront interiors are tab and slot construction, but were difficult to align.



Step 43 was to install the clear plastic windows, but installing the windows after gluing the green building front might be preferable.

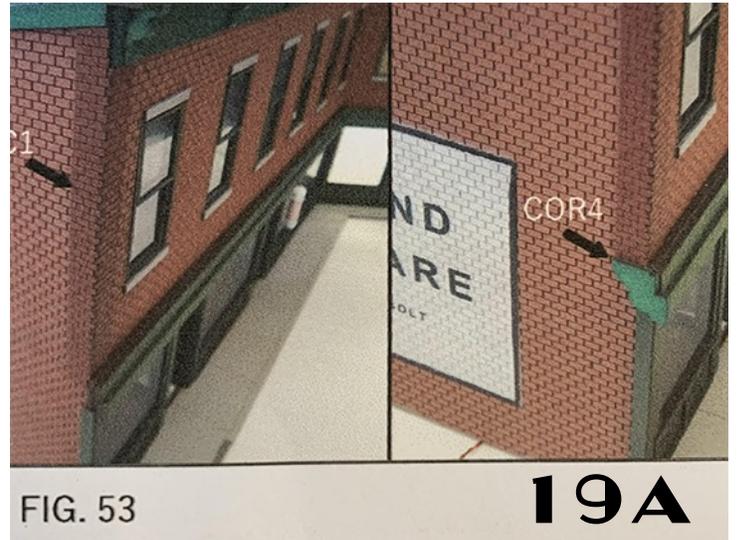
Step 44 in the instructions says to glue the store fronts on, but does not show how to hold something while the glue dries. Three methods of clamping shown in picture 17 are: 1) brass clamps, 2) 3/6" binder clips, and 3) magnets.



C clamps are used to hold the facade trim while the glue dries, picture 18. This picture shows a departure from the instructions. For the first floor cornice, I located the narrower trim at the top. Instructions figures 45 and 53 show this trim at the bottom.

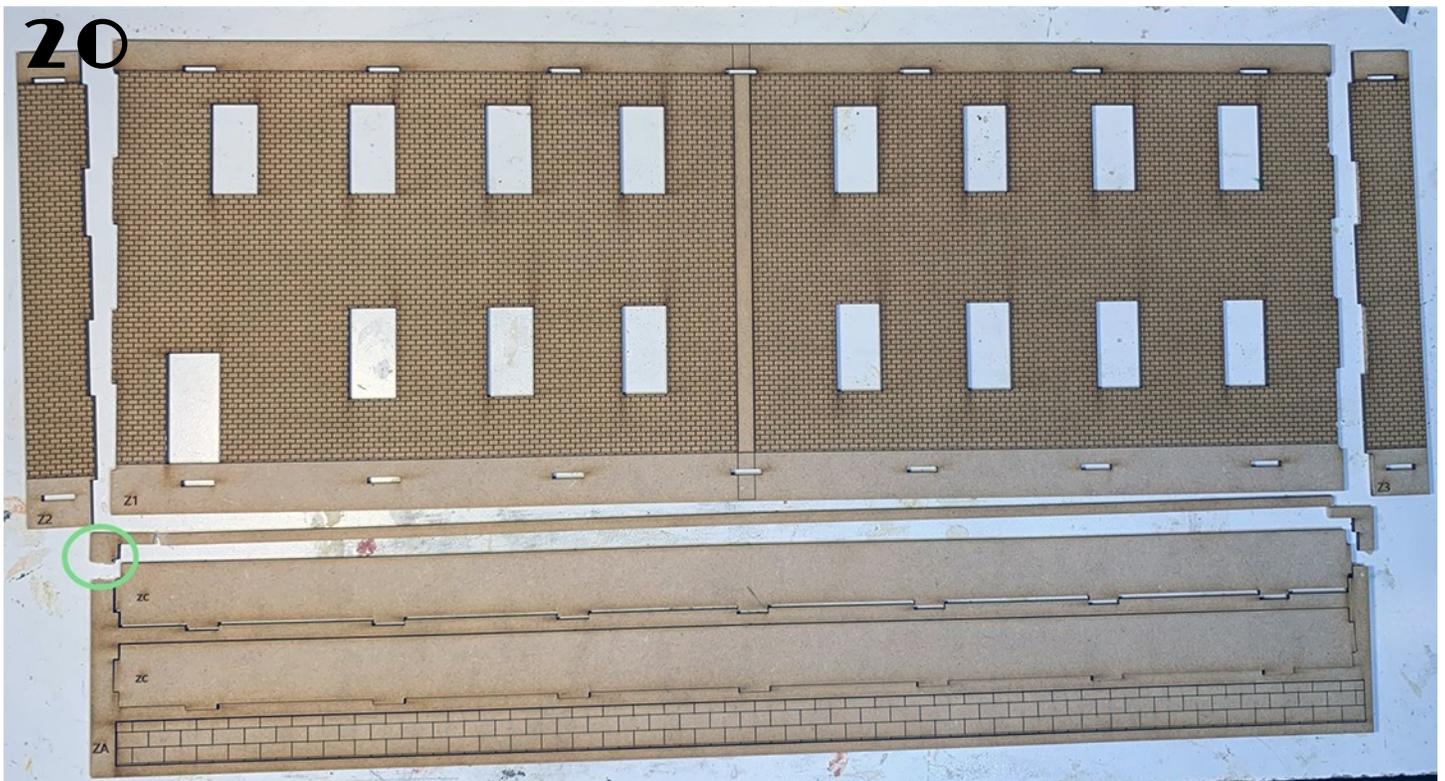


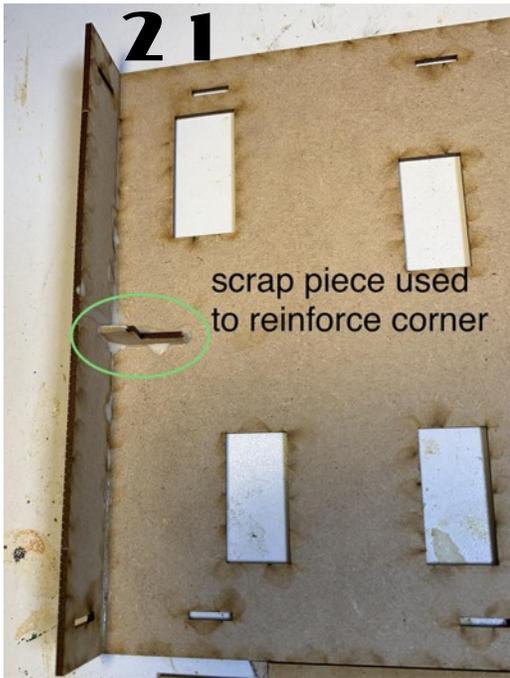
This section of the Nighthawks painting shows the first floor cornice of the background building. The instructions figure 53 seem to be opposite of what's in the painting. Picture 19 and 19A.



### 3) Two Story Flat

This is the simplest build of the three structures, and would be easiest to modify if needed. The structure represents two adjacent buildings separated by a firewall. This unassembled view shows the tab and slot construction, and the green circle points out a scrap piece used for reinforcement later. Picture 20.

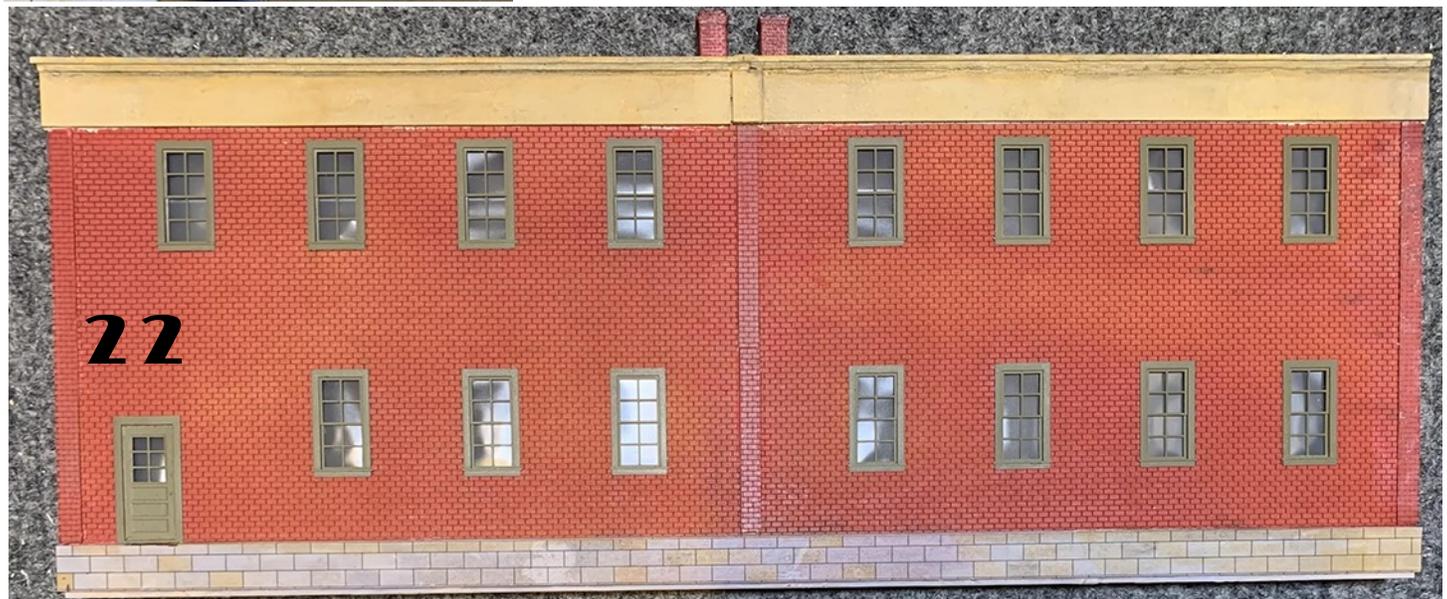




Picture 21 shows an example of using a small piece of scrap for corner reinforcement.

This view of the building, picture 22, flat shows how it's divided by a "firewall". The olive drab windows are more subdued than the white illustrated in the kit instructions. I also centered the two chimneys rather than having them on opposite ends.

The inside view, picture 23, shows scotch tape as window glazing. It's partially opaque and typically doesn't reflect light.



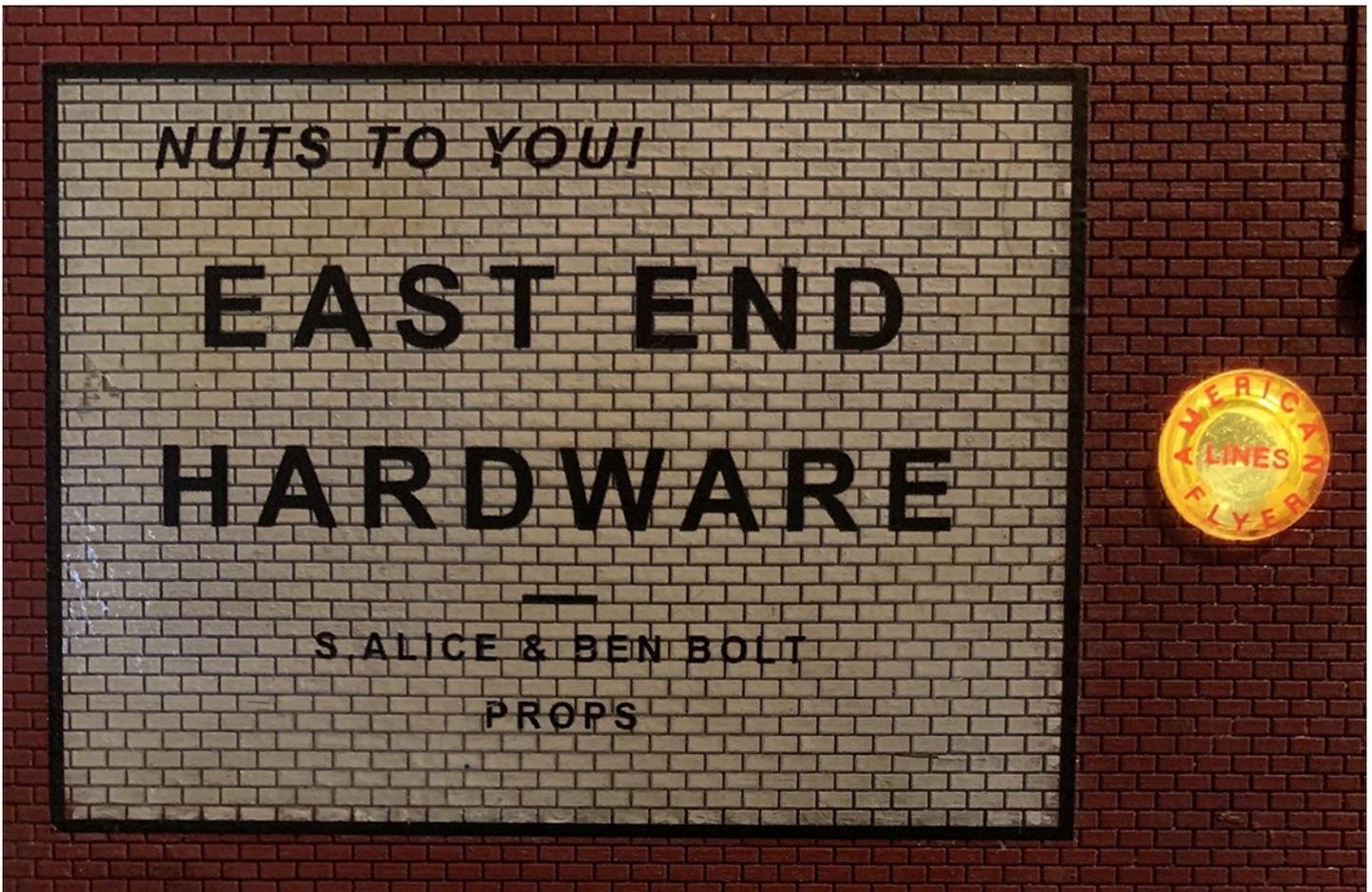


Above is close up of the diner. The kit includes a mirror seen here at the street's end .



Most of the sheet material was soaked with linseed oil, especially the street, sidewalks, and rooftops. The linseed oil darkens the “pavement”, which was further darkened with India ink and alcohol. I also did this to the roof of the hardware store building.

This view, left, shows the scene in a corner on a 40” radius curve. This is similar to the orientation on John Armstrong’s layout. Incongruously, London’s Battersea power station is in the background. It’s a poster from Pink Floyd’s *Animals* album. (See cover shot)



I added an illuminated American Flyer drumhead as a detail on the hardware store wall. Manhattan's Madison Hardware became primarily a Lionel train store of some renown, so I decided that East End Hardware would likewise have a hobby sideline.

This is one of the more complicated kits I've built. It's challenging but gratifying, and I got my money's worth. I had an hour of fun for every dollar spent on the kit, lights, etc, over \$266. I wouldn't want to scratch build something like this, but replicating John Armstrong's design was enjoyable.

Two items that I used are recommended:

<https://www.magcraft.com/magcraft-nsn0911>

<https://www.staples.com/Staples-Small-Metal-Binder-Clips-Bulk-Pack-Black-3-4-Size>

**THE** **O** **RESOURCE**  
NEWS, REVIEWS, INFORMATION TO USE  
**ONLINE MAGAZINE**  
**SCALE**

**Don't forget to read our other magazine, The O Scale Resource, for more modeling ideas.**

NUTS TO YOU!

# EAST END HARDWARE

S. ALICE & BEN BOLT  
PROPS

FREE  
STUFF

GRANT  
HOTEL

LAST ANNUL  
FIREMA  
X OC



HOTEL  
GOTHAM  
'dine d

DOWNTOWN  
THRIFT & LOAN

CATTARAUGUS COUNTY  
N'S CARNIVAL  
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PIERRE  
E  
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# S SCALE SCHNABEL CAR

*S Scale Schnabel Car scratchbuilt by John Griffin on the late Doug Evans layout currently owned and operated by John E. Henning.*

John & Pat Henning showed us pictures of some older Carmel, IN all “S” Shows put on by Charles Malinowski. One thing that really caught my eye was a Schnabel Car. These things are massive in any scale and have so many details to look at. Beautiful build.







The photos with the long load were placed for show and photography. The car with load would not negotiate the curves on the layout. John once said, he never found a layout big enough where the load would go around curves. The photos of the car on a curve is with the train actually running. The train ran around a number of times at the show (minus load).

With the overhang on curves, we had to stop the train on the inner loop to prevent sideswiping. Although, one young teenager, Doug, we were letting run the layout did pull another train out and we almost did have an accident.



## Revisit the

# MINNESOTA HEARTLAND

By Ken Zieska

**Editors note: Amy and I visited Ken and his railroad back in the summer of 2015. You can read that article in the [December/January 2016 issue of The S Scale Resource Magazine](#) here.**

Welcome back for a 2nd tour of the Minnesota Heartland hosted on the pages of *The S Scale Resource Magazine*. It is difficult to believe, but it has been over five years since we were able to host a visit from our S Scale Resource friends. We were hopeful that Dan, Amy, Glenn and many of you would have visited us in person, during the 2020 NASG Convention in Bloomington, Minnesota. Regrettably, we needed to postpone the 2020 Convention; but the Pines and Prairies Workshop is hopeful we will be able to welcome everyone in 2023!

There have been significant route changes and many scenes have progressed since Dan and Amy stopped by in late 2015. Let me start this article with a look at the biggest route change. [On page 46 of the December/January 2016 issue of The S Scale Resource Magazine](#), you will see the track plan that “was”. (If you’re reading this issue on-line, simply click the link above for the track plan.) There is a new track plan at end of this article.

In preparation for the 2020 Convention and to improve operations, I removed all of the track and benchwork in front of Heartland Grain Industries in Marilyn Junction, over to the spot where the track disappeared behind the bluff. This accomplished 3 goals; it allowed me to create a visible staging yard (MVS), it eliminated the existing duck under, and it provided a new area to create a prairie scene. However, it also disconnected the twice-around-the-basement layout concept.

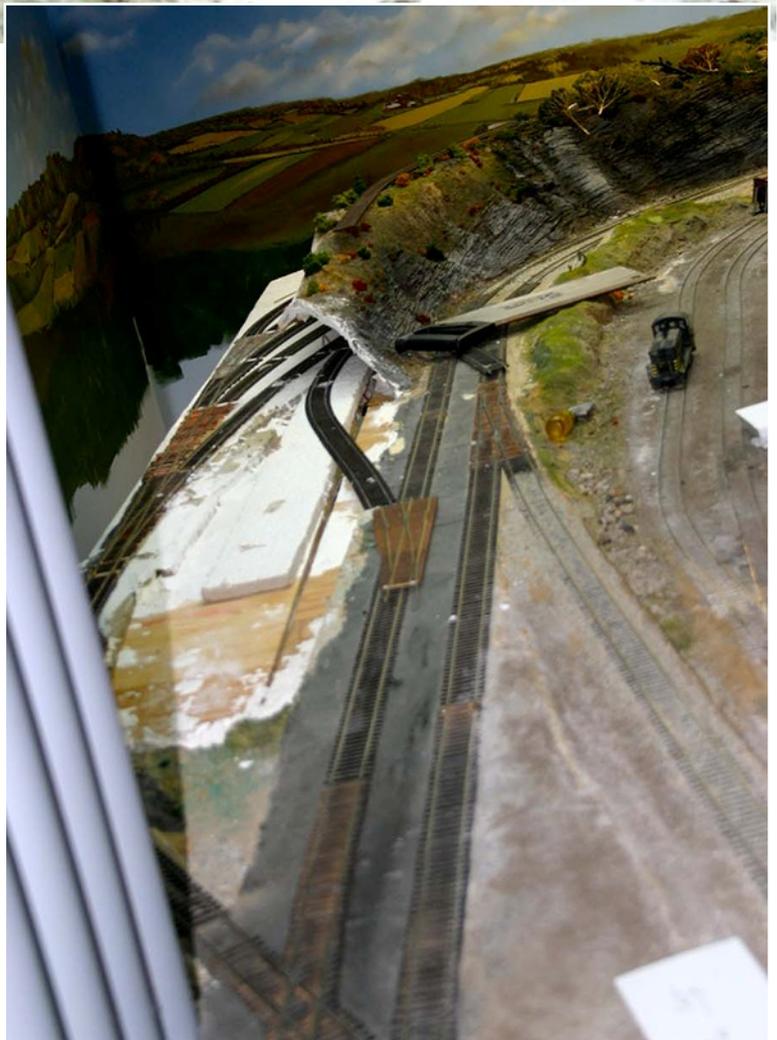


*When I rebuilt the benchwork, the track plan was more simple and gave me more room to host guests. Because this also removed the hill mass in front of the window, the basement gets additional light which is nice.*



*Above: The Northern Pacific train is on the River Valley Line, the original second main was behind the hills in the background. While this was a nice scene, I wanted to create more “prairie”.*

*Right: At the bottom of the photo, you can see the diamond where the original long mainline crossed itself. This was an interesting arrangement with a manual interlock to protect the crossing. During more than one open house, the guest were treated to exciting action when two trains sped toward the diamond while the operator was distracted threatening to create a scene right out of the Addams Family.*





When you last visited, the railroad made two loops around the basement. Eliminating the duck under and creating the visible staging eliminated the direct connection to the lower loop. This created essentially 2 layouts, with a junction between them. One, the old upper loop, is a circle around the basement. It has opportunities for switching, but most important, is a great train watching route. The other line is the principal operations route and essentially operates as a point-to-point between MVS and destinations. It has proven to be a great trade off, the one long continuous loop supports limited operations at a few industries along the way to Asuncion, while allowing me to enjoy watching trains and enjoy great viewing locations along the mainline. This enjoyment has been made better now that I can share train watching with our Grandson, Cruze.

Looking at the new Marilyn Visible Staging (MVS) yard. This is a stub end facility with plenty of +track space, where I stage trains for operation sessions, and to make and break trains during the sessions. I will make no attempt to scenic this yard, it is an operations tool. Immediately behind the yard is a small park area where rail fan campers can set up their rigs and enjoy the great early fall weather and watch trains. As you can see, this is also a favorite place for wildlife to gather.

*Sharing the railroad with friends, and now our Grandson, makes the hobby even more enjoyable.*

One of my favorite source for wildlife figures is MiniPrints.ca. Today, we depart with the Northern Pacific freight (363-Asuncion Turn) running as extra 309, leaving the staging yard.

*MVS is a nice long stub end yard which gives room to stage the first group of trains for an operations session. The MVS crew will continue to accept returning traffic and build additional trains for the “afternoon” turn that many of the jobs require. Note there are numerous cross overs on the Arrival and Departure on the left side of MVS.*





*Above: The Northern Pacific “Asuncion Turn” is one of the longest trains we run so it has been built on track 9. Our power is ready, the air brake test complete, we are ready to roll. The train leaves as Extra 309, the number of the first unit. Below: Our train has just passed through hidden storage and is now in Cannon Valley. We cross the small bridge to follow the Runansota River Valley to Prairie Junction.*



From MVS the single track Northern Pacific Industrial Line heads west, passing through DuLoo. The track ducks behind the shower and enters Dryer Junction. Here at Dryer, the Minnesota Heartland track branches out toward CentralSota on the Minnesota Prairie. But our train will continue west and past the Northern Pacific 27th Ave NE Junction. This industrial area is as busy as any other on the NP. They regularly run 4 separate jobs here each work day. Past 27th Ave. NE is the wye into the Downtown Alley. The “Alley” is another busy location for interchange traffic and services the Downtown Warehouse District. The

train ducks out of sight for about 30 feet, passing through hidden storage tracks, before popping out to follow the banks of the Rumansota River. From there it continues to rejoin the Northern Pacific mainline at Prairie Junction.

We ease to a stop at the NP Junction switch, and when cleared by the Dispatcher, we proceed out onto the main. We soon arrive at Heartland Grain, an important year round customer for the NP. We pull 3 loaded hoppers and spot 3 empties before proceeding west. We are traveling on the upper level now and soon we encounter a long, but gentle grade, that takes us up to the town of Asuncion. In Asuncion, we work Glacier Produce and Bosch Beverage. We often set out blocks of cars in Asuncion for pick up by through trains and often find cars they have left for us to spot or take home. When we are done, we run around our train and pump up our air to return home as train 364, extra 6303. Because no one is ever in any hurry when you visit here, crews often take a few extra laps around the basement for the rail fans before they leave the main at Prairie Junction and head for MVS.



*Our train eases to a stop waiting for clearance onto the NP Mainline at Prairie Junction. It is a nice place to stop, although by mid summer, there have been know to be a few mosquitos in the air.*



*En route to Asuncion our train stops to work Heartland Grain. The loads we pick up are often left in Asuncion to be picked up by the through freights traveling up to the port of Duluth.*



*Bosch Beverage in Asuncion is a good customer for the Asuncion Turn. We deliver grain, corn syrup and packaging supplies and pick up boxcar loads for distribution throughout the Midwest.*



Let's take a second trip, this time to the Northern Pacific 27th Ave NE "F" Line. First, take a break at DuLoo and grab a brownie in the crew lounge. We will again leave on the NP Industrial Line. Our head end brakeman drops down and opens the switch taking us off the Industrial Line at 27th Avenue, NE. As soon as we clear the switch, we stop and the rear brakeman locks the switch to the normal position while our conductor goes over to the bill box to see about a couple of cars that have been left on the interchange track. The cars on the interchange are for Northern States Power (NSP), an "F" Line Industry. The Conductor will determine when he will fit that work into our shift today.

*The Northern Pacific "F" Line job is shown leaving the Industrial line at 27th Ave. In the Twin Cities, the NP has divided its trackage and identified it with letter designations. For example, the "A" line is the passenger main, the "B" line the freight main. While the "F" line is a busy industrial area, we are not talking mainline action here.*



*Above: When our cabooses clear the mainline, the train stops so our conductor can check the bill box to find out the destination for the cars on the interchange tracks.*



*Left: The Creamettes name has been a part of the grain processing business for many years. We deliver grain and processing supplies and pick up their great Pasta products for shipment across the country.*

The switch list directs the work to be accomplished on the assigned job.

The form will be completed with the assigned engine number, time, date and origin of job.

Class indicates car type. A simplified AAR code is used to assist the crew in car identification only.

Initial indicates car ownership for crew assistance.

Number indicates the car number for crew assistance.

The entries on the top of the form indicate the consist of the train in the yard. It is the crews responsibility to confirm the consist of their train. The location in the "TO" column indicate the delivery location.

Cars that have been previously spotted needing constructive placement or movement to another location on line will have industry track numbers in the from and to column.

Cars to be picked up and returned to the yard will have the industry track number in the from column.

Work that cannot be performed will be noted on the switchlist and returned to the yardmaster.

*We use switch lists patterned from those used by the NP, but simplified for use by Model Railroad operators. The cars entered on the top of the form represent the cars in our consist. Each line has car type, initials, car number and the spotting destination in the "to column" At the bottom of the form are the pick ups we are to make, again listing car type, initials, car number and pick up location. To see an example of the map the switch crew works from, keep reading.*

**LOCAL SWITCH LIST**  
SWITCHLIST OF ALL WORK TO BE DONE BY CONDUCTOR

TRAIN NUMBER NP 047 A

AT \_\_\_\_\_ STATION \_\_\_\_\_ 19 \_\_\_\_\_

	CLASS	INITIAL	NUMBER	FROM	TO
1	LO	D+NE	9275		F51
2	XM	MILW	22187		F41
3	LO	CBQ	85121		F42
4	LO	MH	579		F42
5	TM	CGTX	1096		F21
6	XM	GN	47678		F33
7	LO	MH	573		F32
8					
9					
10					
11					
12					
13					
14	LO	M+StL	76499	F51	
15	LO	CB+Q	85102	F42	
16	XM	MILW	8900	F41	
17	XM	CP	249391	F22	
18	TM	SHPX	10761	F21	
19	XM	SHRB	2500	F33	
20	XM	MH	899	F33	



Today will be a full day. Be glad you got that brownie to tide you over, frankly there is a good chance they will all be gone before we get back. Jamie Hanson is here tonight!

*Industrial trackage needs to be a bit creative to fit the space available. Here we see the railroad needed to cut the switch in on the second track to be able to make the curve into Standard Distribution. This curved turnout was built using the Lincoln Log method Dick Karnes explained in the [December/January 2021 issue of The S Scale Resource](#).*



*This crew has delivered to the NSP Power Plant and is now pulling empties. The red stop sign brings them to a complete halt until they can safely cross the two track Downtown leads. The “F” Line crew enjoys no priority at this crossing.*

Some interesting features of this line include the curved crossing into Standard Distribution and the crossing of the double track line to Downtown. Our conductor determined that he will work the industrial customers before going back after the interchange cars for NSP. Things go smoothly soon we are ready to work the NSP cars on the interchange track.

Because the downtown line is busy, we must come to a full stop to await clearance before proceeding across the diamond to NSP. That complete, one last piece of business, a car from Sunshine and one from Standard Distribution need to be spotted on the interchange track as they are going off line. We set them on the interchange track for pickup by a later transfer run. Finally, we make up our train and approach the 27th Ave switch to request clearance to return to MVS.

*Our work complete, all the deliveries made, the pickups collected and the cars for transfer set on the interchange track, our crew waits on the 27th Ave Switch for clearance to enter the Industrial Line and return home.*





*Here is a photo of a busy street scene in CentralSota. We can see two boxcars that have just been delivered to the COOP waiting for the elevator crew to inspect the cars for cleanliness and install the grain doors before they are pushed over for loading.*



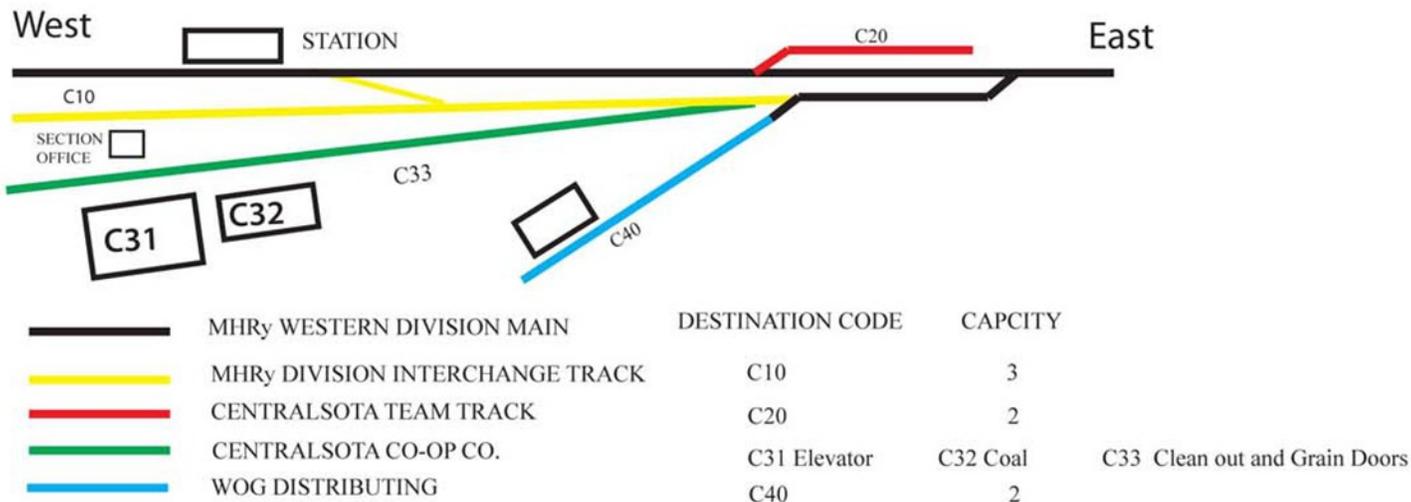
*Another view of CentralSota. We can see the station on the left side of the photo and the COOP elevator on the right side. In the center of the photo are the maintenance facilities for the Heartland. CentralSota is the Division Point for the railroad.*

Our day is not over, the third run will be on the Minnesota Heartland. If you thought this was going to be an easy day, you're out of luck. The easy day was yesterday. Immediately, you're called to ride the Minnesota Heartland freight to CentralSota. You glance over at the crew lounge and see nothing but crumbs in the brownie pan, you cast a glance at Jamie and Ben, they just look at you and gesture "What?". Well, just grab a few chips and hop on. You pass through DuLoo again and get a friendly nod from another crewman just hanging out. In Dryer, your train diverts onto the Minnesota Heartland tracks. The conductor turns down the volume of the train radio on turns on WCCO to hear the Twins game. The Heartland is dark territory, no dispatchers and their schedules to worry about. Life on this Granger short line is pretty laid back. The train abruptly stops and you look up, the conductor suggests that the head end brakeman had been riding the front porch of the locomotive and may have bagged a pheasant that was loitering on the mainline eating corn droppings. We soon rumble past Grey Eagle, we work them on the return trip and continue on to CentralSota.

We are traveling with empty boxcars for the grain elevators, a boxcar of merchandise for Wog Distribution, and a carload of bricks we will spot on the team track. We also have a couple of cars to drop on the interchange tracks where the turn from Lake Wobegon will pick them up for delivery to the Great Northern.

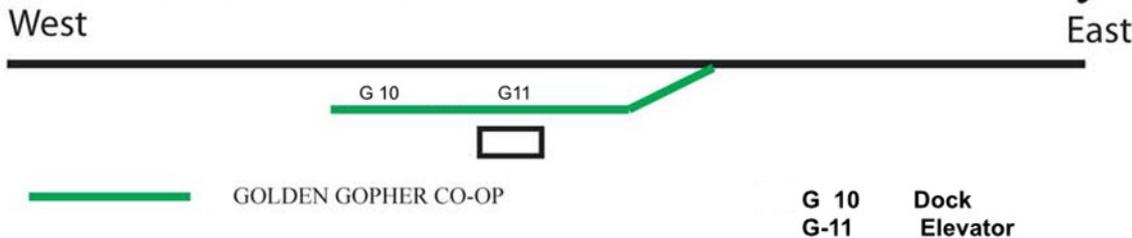
# CENTRALSOTA

MHRy MP 151



# GREY EAGLE

MHRy MP 134

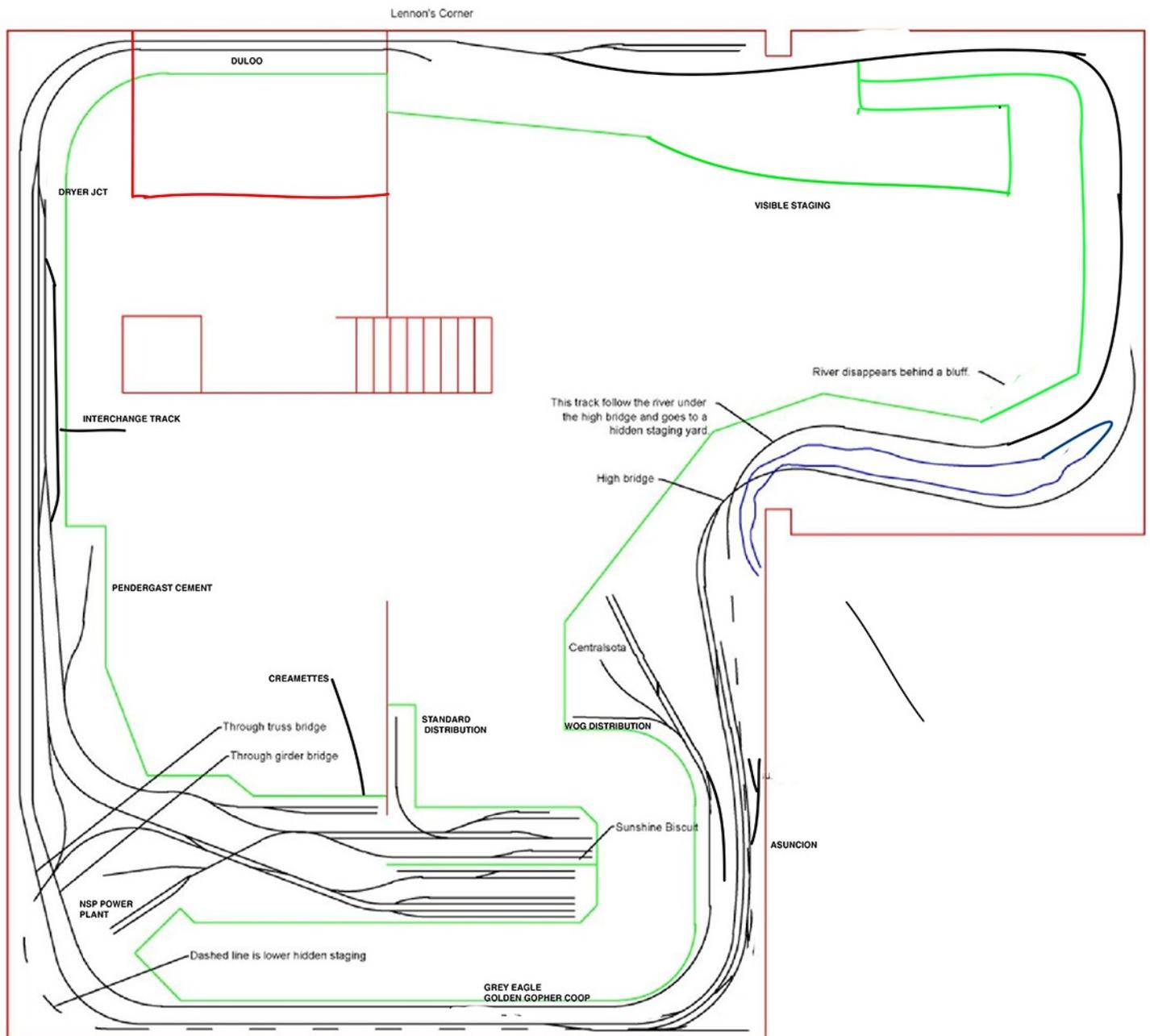


## MHRy A.M CentralSota

*This is the track map the crews have for working their assignments. It is pretty straight forward, it shows the tracks, track capacity and spotting numbers on the track. These numbers are listed on the switch list making it easy for the crews to identify their daily work. Most leads have a bit of extra head room to help with local switching however crews must carefully consider how they go about their work to not get themselves all tied up.*

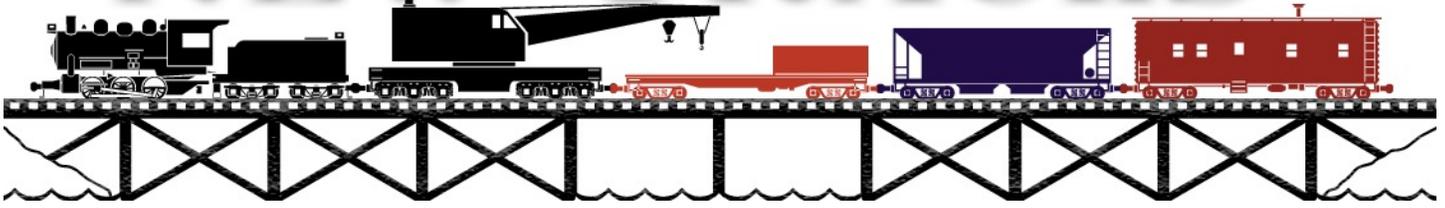
You pull the loads from the grain elevators and spot their empties. The elevator crew jumps right on them, opening the doors for grain loading, then pushing them to the elevator. Time is tight, we can expect both cars we spotted will be loaded before the day is out and we know the Heartland will send another grain extra out to pick these up and leave fresh cars for late night loading. We switch out Grey Eagle on the way home where they also get busy on the empty cars we left them so they are ready for pick up later today. The Twins won today. Jim "Kitty" Kaat pitched a gem, Harmon "Killer" Killebrew hit a towering homer to center and Zoilo Versalles turned in some defensive gems so before we reach Dryer Junction, the train radio volume is back up and we request clearance onto the Industrial Line.

I hope you have enjoyed your visit. I admit that I had not thought that much had changed until I started looking over the photos from 2016. Make sure you put the NASG Convention in 2023 in your plans and make sure you stop by for a visit. Now that you are a veteran of operating here, you should know that operations are never in a hurry. You could have stopped anytime to get a second brownie before they were all eaten.



*Basic track plan of Ken Zieska's Minnesota Heartland Railway*

# NEW TRACKS



Mentor Definition: A Trusted Counselor or Guide

By Contributing Editor Jim Kellow MMR



## MENTORS WITH DIFFERENT SKILLS PRESENT DIFFERENT LEARNING OPPORTUNITIES!

### Where Mentors Help Modelers Build

#### New Tracks" Announcements:

First, I want to ask you to please visit our new website [newtracksmodeling.com](http://newtracksmodeling.com) to get the latest information about what we are planning. Also, make sure to register, and confirm your registration, by replying to the email you will receive. This will make sure you get notices of all our future zoom "New Tracks" events and zoom log in links. This enables you to personally participate in our discussions and not miss any of our "Build Along" kit building and other special projects.

Please also send the zoom log in link to your friends so they can also join in the fun and be a part of our shows. Thanks in advance for hour help and support.

Second, we have four "Build Along" events featuring talented modelers. Two are building S Scale model kits. The kits are also offered in other scales, but these modelers selected S scale for their builds. One of these "Build Along" S scale projects started on April 17 and the other is scheduled to start on May 22. We have a third "Build Along" by a modeler which is also starting on April 17 and another on our June 2 show. They are:

1. Gaylord Gill and Randy Bosscher, both experienced S scale model-builders, are building a model using "The 1-Kit" by Bar Mills Models. The kit is available in N, HO, S, and O scales. A 25% discount is available from the Kit manufacturer for modelers who want to participate in this BUILD ALONG project. Order the kit at [barmillsmodels.com/product/the-1-kit-n-ho-s-o/](http://barmillsmodels.com/product/the-1-kit-n-ho-s-o/) so you can BUILD ALONG with two great mentors Gaylord Gill and Randy Bosscher.



The initial introduction of this project was on February 27, and the first construction session was on April 17, 2021. You can view a video of these segments on our [New Tracks Modeling YouTube channel](#).

On May 29, there will be a Special "My Build" segment on our show where modelers will be able to show their model and discuss building it from the Bar Mills 1-Kit. There will be a raffle for the modelers who show their models, with the winner getting a \$25.00 discount certificate from Bar Mills to use on the next model they purchase. Thanks again to Bar Mills for their support and interest in this project.



2. Rick and Maureen Hunter will be building and weathering their Hunterline kit of a 30' King post Bridge. The kit is available in N, HO, S, and O scale. They will be building the S Scale version.

This BUILD ALONG will be presented in eight (8) weekly, 30 minute segments starting on our "New Tracks" Meetup May 22, 2021 at 7pm Eastern Time. If you have missed a segment, you can see a video of it on our [New Tracks Modeling YouTube channel](#).

Join Maureen and Rick from Hunterline, as they take you through, step by step instructions, to build your own Hunterline 30' King Post Truss Bridge in your scale (N, HO, S, O, including all the Narrow Gauge scales).

Skills to learn: read templates and instructions, distressing basswood, staining and gluing techniques, assembly, drilling for rods, NBW installing, finish weathering techniques. This is a beginner to intermediate difficulty. Old pros can learn a few things too!

The price includes the kit and two bottles (8oz) of Hunterline Weathering Mix – a base colour, and a highlight colour. The highlight colour Rick and Maureen recommend is their Creosote Black. They will advise you on the base coat if you tell them the weathered look you want to achieve.

"New Tracks" Meetup Special pricing includes the Kit, 2 bottles of Weathering Mix & Shipping cost to your address: N - \$65.00USD HO - \$68.00USD S - \$72.00USD O - \$80.00USD

Recommended Tool List to build the model:

- Work Surface - cutting mat - big enough for your scale
- Leftover dish for stain
- Wood glue – fast tacking carpenters glue such as Titebond Regular or II, Elmers or LePages
- Masking tape
- Waxed paper
- Pin vise (drill bit is included in the kit)
- Razor saw and/or Xacto knife
- Tweezers – very sharp point is helpful
- Wire snips
- Small files, clamps, cheap brush
- Disposable gloves
- Patience

Please order your Hunterline Kit and don't forget to Mention "New Tracks" to get the special discount. Phone: 1-866-934-4174 or preferred Email: [rick@hunterline.com](mailto:rick@hunterline.com). For more information visit the [Hunterline Website](#).

Just a suggestion on building Rick and Maureen's "Build Along" Model Bridge. Al Collins makes a sanding tool that, based on the video operation of the tool I saw, if I was going to build this kit, I would contact Al at his [Ultimation Website](#) and ask to look at the video of his tool in operation. Al is offering a 10% discount to modelers building the Hunterline kit in the "Build Along".



3. This "Build Along" also started on April 17 by Modeler Bob Farquhar, a very talented modeler, is the Blair Line "General Store" kit, with kit designer Dale Rush. This kit is available in N, HO, and O scale. Sorry no S scale version. A 25% discount is available from Blair Line for this BUILD ALONG project. A special My Build segment for those modelers who Built Along with Bob will be held on the May 29 "New Tracks" Meetup .

Bob presented the first segment on this BUILD ALONG February 20, 2021 which discussed the tools, paints, and glues needed to build the model. This segment is available for viewing on our [New Tracks Modeling YouTube channel](#).

Go to <http://blairline.com/> to order the kit in N, HO, or O scale, and be sure to use the code "newtracks" to receive a 25% discount and free US shipping. Order the kit so you can BUILD ALONG with your mentor Bob Farquhar.

4, Bill Davis is starting to build a Labelle Woodworking Open Platform Baggage Car kit number HO3 in HO scale and in O scale O352. He is starting this build on our June 2 show and is scheduled to complete the build on our July 7 show. If you miss any of the episodes, you can see them on our [New Tracks Modeling YouTube channel](#). Labelle Woodworking is offering a great discount of 25% for modelers buying this kit if you say it is for "New Tracks".

On the same shows, Jamie Bothwell is going to demonstrate modeling of Passenger Car Underbody Detail. We are fortunate to have found Jamie, and his presentation should really be of interest to all of us who build passenger cars.

Please join our "My Build" show on "New Tracks" Meetup May 29 to see the models built by modelers who "Build Along" with Bob Farquhar, Gaylord Gill and Randy Boscher. Also join us July 24 to see the models of Rick and Maureen Hunter "Build Along" and Bill Davis "Build Along" participants.

I am interested in getting more S scale model builders and/or manufacturers involved in future "Build Alongs" starting in the August/September time frame. If you are interested, please let me know at [jimkellow@sscaleresource.com](mailto:jimkellow@sscaleresource.com). Remember, the model builder can select the manufacturer and specific kit they want to build. This program seems to be working to getting modelers who have not built anything for years building again. It is really great to hear their enthusiasm. Thank you for your input.

Third, we are starting several new segments.

The first is "Watch Me Build" which started 4/28/21 with the first modeler, John Frankforther, a life long American Flyer/S Scale modeler building an S Scale scratchbuilt bridge. Please come and see John's artistry.

This new segment can be building a scratchbuilt, kitbashed, or kit model. The purpose of this segment is for you to show your modeling skills and techniques so others can benefit, and for you to get constructive comments to help your future modeling.

You may never have shown your modeling before in public for a variety of reasons. I assure you, I will do everything possible so you will enjoy and benefit from participating. This is a new segment and it will evolve over time, so please contact me with your interest and help me develop the segment. My email is [jimkellow@sscaleresource.com](mailto:jimkellow@sscaleresource.com) If you would like to discuss your idea by telephone, let me know in your email, and I will provide you my number or you can get it off the website.

Second is a segment about building old kits. It is called "Remembering and Building Old Kits", and will have modelers building kits from our distant past that are no longer manufactured and hardly available. Kits whose names we may have forgotten, but when we hear their name again, bring back great memories from our youth and remind all of us what modeling used to be like. The first two segments are being scheduled and will be announced shortly. As with our Build Along segments, these will also be recorded and available on our "New Tracks Modeling" YouTube channel. I hope you tune in to our Zoom shows and check them out.

Third, because I won a prize at the recent NMRA Pacific Coast Region Digital convention, I met Anita Walter who owns Central Coast Trains store in California. I must admit it has been a very long time since I have been to a train or hobby shop. In talking with Anita, a lot of old memories came to mind about how much fun, and education about modeling, I used to get from going to my local hobby shop. So after getting positive comments from viewers on my show, I decided to start a new series called "Going to the Hobby Shop".

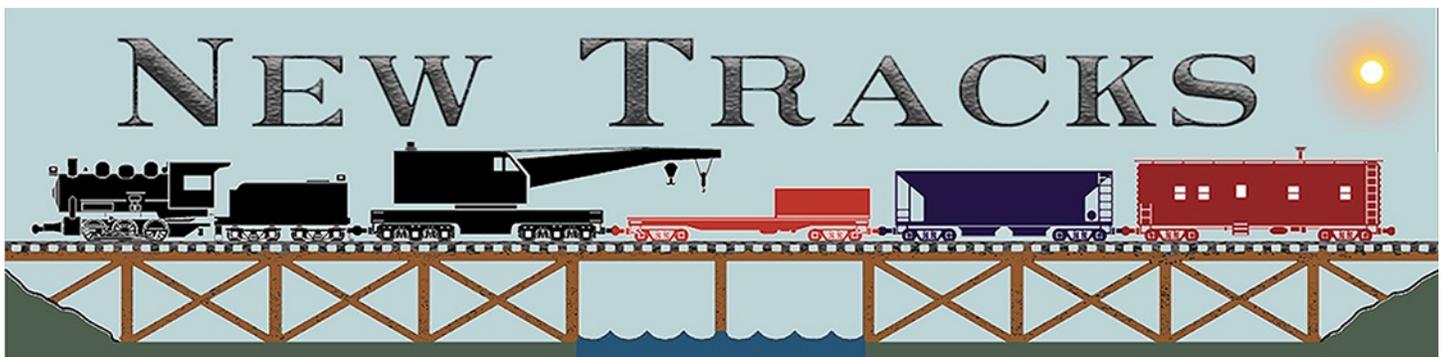
Anita will be my first train store in the series and will be on my show June 9. I hope you will join us to meet this lady who brought back so many great past memories of past hobby shop visits for me. You have got to hear about the really special model railroad her husband built.

If you have a hobby shop you would recommend I ask to be on my show, please let me know. There are not many hobby shops left around the country, and the ones that are trying to serve modelers, I believe, need to be recognized and supported. So please tell me about your hobby shop at [jimkellow@sscaleresource.com](mailto:jimkellow@sscaleresource.com) and I will ask them to be on our show.

## Show us Your Modeling

We have a monthly Special "My Build" segment on our show where modelers are encouraged to show models they are building and discuss their modeling techniques. The next one is scheduled for July 24. This "My Build" will have a special segment for those modelers who build the Hunterline Bridge and the Labelle Woodworking Open Vestibule Baggage Car by Bill Davis.

[Hunterline](#) is offering a drawing for modelers who show their bridge model, and the person drawn will receive a \$50.00 discount on their next Hunterline purchase. The modelers who show their model of the [Labelle Woodworking](#) kit will be entered in a Labelle Woodworking drawing with the person drawn getting a free Labelle Woodworking kit of their choice



More "Build Alongs" coming soon.



Starting Oct 16, Bill Davis is building a [Rail Scale Models](#), Tobacco Barn Kit (Dan put in photo from website and also a link to the . website) on our show. Stephen is offering a 20% discount on the kit for modelers who want to Build it along with Bill Davis. The discount code is now set up for the Tobacco Barn in all four scales (N, HO, S, O) for 20% off MSRP. The buyer must use the coupon code "NewTracks", one word, at the check-out screen. This code will be in effect from June 1 thru Oct 30, 2021.

Martin Brechbiel, MMR starts building a [Leadville Designs](#) Maintenance of Way kit on our September 22nd show. There is a great discount offered by Leadville Designs on the kit if you mention "New Tracks" when you order.



Earl Hackett is going to show us how to make 3D parts starting on our June 9th show and finishing on our June 30 show. If you have ever wanted to make a 3D part and have it printed, don't miss these shows.

Paul Egri has designed a card model of an Engine House that can be downloaded for free so you can "Build Along" with a very talented card modeler, Dave Rarig, starting on our July 17 show and ending on our August 7 show. It doesn't get much cheaper than this to build a model. The model will be posted in the near future on our website [NewTracksModeling.com](http://NewTracksModeling.com).

I have posted a one page card model you can download now for free in any scale, and use to test your card building skills, on my Jim Kellow Facebook Page [Jim Kellow MMR](#).

Jim Murphy will be building a Labelle Woodworking HO "Business Car" kit starting September 11. Order either the HO or O Scale Business Car saying it is for "New Tracks" to receive a great discount.

Starting on our Oct 20 "New Tracks " show, a Motrak Models kit will be built in 4 different scales by 4 different talented modelers. The build will continue once a week on our show until it is completed. Thanks go to Jeff Adam who owns [Motrak Models](#) for making this event possible. More details on the kit and the great discount Jeff is going to give will be available on my [newtracksmodeling](#) website. If you want to see how a model is built in your scale compared to how it is built in other scales, these shows will be for you. Plus, you will see the various modeling techniques used in the various scales. Thanks go to Phil Edholm for suggesting this unique concept. I think this is a great idea and hope you will want to participate.

Bill Banta of [Banta Modelworks](#) is teaming up with Clark Kooning, MMR for a "Build Along" starting on our December 1st show. More details will be available closer to the date of the build on our [New Tracks Modeling Website](#).

Chris Coarse of Conowingo Models is teaming up with Geoff Bunza to build and animate a model kit Chris is working on. This is something different and more details including a start date will be available on our website, [NewTracksModeling.com](#).

I hope you want to participate in all the "Build Alongs". The modelers and manufacturers who are making these events possible are doing them to try and help you improve your skills and improve and have more enjoyment from your modeling. Please show your support for these events by your active participation, suggestions and ideas. Thank you.

### **Now let's hear from the S scale winner of Evans Software Contest Drawing**

Before I start this portion of the article, I want to share some comments from the Evans Software contest Drawing winner, Terry O'Kelly. I think you will find them interesting.



“Hi Jim, I apologize for getting back to you so late. The end of last year was busier than I ever expected, and almost no time was spent on trains, unfortunately. I did get the software and experimented with it a bit. My layout has been in a fairly finished stage with little room for new structures. There was one part, however, that had been a problem; and ModelBuilder software helped.

The layout runs around a small 15 x 11-foot utility room – the rest of the basement was dedicated to my daughters' birthday parties, dancing, and toys. One corner of the room has the electrical panel and breaker box that looms above any large building I would have placed there. The panel needs to be hidden and still quickly accessible if a breaker trips or I have to do some work on lights somewhere. My solution was to have a paper backdrop suspended by

magnets that would sit behind a removable plywood panel. I glued a few buildings, roadway, and scenery to the plywood panel so that nothing would be disturbed when I lifted the panel from the fixed wood framework. Once that panel is lifted out, it's a simple job to pull the backdrop/magnets from the electric box to gain access to any breakers.

[ModelBuilder](#) software allowed me to design lightweight paper overlays that I glued to the paper backdrop to make a more interesting background. One building actually wraps around a two by four so that particular building appears three dimensional. The scaling of the software allowed me to size the buildings so that their dimensions could be changed to represent the correct perspective in relation to their distance relative to the buildings in front or "behind" other buildings. In the scene below, the beige and reddish buildings are glued to the plywood panel and everything behind is the paper backdrop. The two ModelBuilder designs are the two brick buildings behind the plastic versions in front of them. The brick building on the right is the one wrapped around and hiding the wooden stud.

There is another wall that is only half done at this point. My plan was to have everything done by Christmas. Now maybe the fourth of July is more reasonable. That wall will have a fixed backdrop set behind some plastic building fronts. I will have some ModelBuilder designs behind the fronts to add dimension to the backdrop. I realize that the software is ideal for building entire structures, but I have found that it has helped me



come up with creative solutions to what I had considered major problems. I just wish I had more time to spend with it...and with trains.”

I shared Terry's comments with Evans Design Software and this is their reply. I found it informative so I have included it here.

"We have had such great feedback from customers regarding our software (as well as our other products!), and we're glad that ModelBuilder was the solution to Terry's design issue. If you look through our ModelBuilder gallery on our website, you can see many, many other applications (not just building train layout structures) for which our customers have used our software."

Thanks to Terry and Evans Designs for their interest and help and interest in this project.

Now lets meet some Talented Model railroaders with different skills who present different learning opportunities!

### **Gaylord Gill** (Photos by Gaylord Gill)



Gaylord Gill is a long time S scale modeler who volunteered to do a "Build Along" on my "New Tracks" twice weekly Zoom Meetups. I really appreciate his help and interest in helping other modelers improve their skills and techniques. Gaylord is currently doing a Build Along with Randy Bosscher on my "New Tracks" Meetup shows. They are building an S scale model. Please meet a very talented modeler.

My imprinting with trains occurred when I was a toddler during World War II. My dad was stationed at Aberdeen Proving Grounds in Maryland, and my mother and I made numerous train trips back and forth from our home in Michigan. In my earliest attempts at artwork with paper and pencil, they tell me I was usually drawing steam locomotives.

My involvement in the hobby followed a very familiar pattern: dad buys son a train set, dad builds son a layout, son works on layout until about age 13, then everything gets dismantled and packed up for a couple decades. It was the late 1970's when I returned to the hobby (by then I was in my mid-30's), and this time I was all-in. I started attending train shows, studying all the scales which hadn't been apparent to me when I was a kid and buying used copies of *Model Railroader* and *Railroad Model Craftsman*. I remember being blown away by photos of what the master modelers of the day were creating.

That first layout my dad had built featured American Flyer track and equipment. S scale always felt like the perfect size to me, and in 1979 I hooked up with a newly-formed club, the Southeastern Michigan S Gaugers. It seemed natural that I would build another Flyer layout, but then at one of our club meetings we had a guest: Ron Bashista of a new company called American Models. He showed off his pilot model of an S scale FP-7 diesel he had developed.

The locomotive was gorgeous. Unlike the oversized, clunky wheels and couplers that marked American Flyer rolling stock, Ron's engine had scale wheels and couplers, and it ran on DC like other contemporary equipment. It was done up in Tuscan PRR livery with the five gold Pennsy stripes, and I immediately ordered a pair of them. When I got home, I repacked my Flyer stuff and eventually sold it. I was on a path to true S scale modeling.

Joining the SMSG club was the best step I ever took. There were, and are, great modelers in the group, some of them converts from other scales, so no matter what questions I had, there were likely to be good answers among the club members. Another key step that rapidly advanced my skills was when I brought in some expert help for building my first S layout with code 100 flextrack. I had gotten to know Joe Kocsis, an MMR who lives in the Detroit area, and I hired him to work alongside me on Saturdays as we did some of the benchwork, trackwork and wiring. Joe showed me a lot of good techniques, so that after a few months, I was ready to fly solo. Going forward, I then organized a group of four of us who were building S layouts, and we



*This model of tower FW in Buffalo was a mostly scratch-built job (see March 2021 RMC). Gaylord sought help from friend Jamie Bothwell to laser-cut and etch the walls and window frames in styrene. The model took Best in Show at the 2019 NASG national convention.*



*Blue Moon diner serves the Pennsy employees working the yard at Olean, NY (see the Jun/Jul 2020 S Scale Resource). Gaylord enjoys creating unique structures – this one was built up from O scale trolley parts by East Gary.*

did round-robin work sessions at each others' homes.

My current Buffalo & Chautauqua layout is my third, and for the first time, I've moved from a freelanced railroad to a prototype setting: my layout represents the PRR's Northern Division in 1953. Centered on Babcock Street yard in the city of Buffalo, it has the Buffalo Main running southeast to Emporium, PA. Running southwest out of Babcock St is the Pennsy's Chautauqua Branch as far as Brocton, NY. The 320' mainline has passing sidings plus double reversing loops at each end, and two towns and an industrial complex provide ample switching opportunities for operating sessions. [Full specs and a set of ten photos are here on the website of the National Association of S Gaugers.](#)



While I've learned a little about all facets of the hobby, I most enjoy creating structures which are unique. In particular, I get a kick out of "bashing" components meant for some other scale or purpose, and creating a structure that's different than anyone else's. Even with kit-builds, I seem to always add or modify something to make the structure my own.

Throughout my modeling years, I've tried to pay it forward by sharing what I've learned. Along the way, I've conducted clinics at SMSG meetings, NASG conventions, and NMRA regional conventions. I have also continued to hone my skills by participating in a regular email exchange with seven other very accomplished S scale modelers. In our exchanges, we share techniques we've learned, photos of projects we're doing, and answers to questions posed by others. It's a great way to both teach and learn.

The New Tracks Modeling program offers a forum for an even broader sharing of model-building tips and techniques.

Thank you, Gaylord, for all you help and support. You can contact Gaylord at [Gaylord.Gill@sscaleresource.com](mailto:Gaylord.Gill@sscaleresource.com).



*Top: Jerry's Garage is a scratch-build which Gaylord contributed to his S club's display layout. He built the doors from sheet styrene, used Pine Canyon parts for the interior details, and finished the exterior to simulate peeling paint.*

*Center: Gaylord created Randy's Bar & Grill, using The 1-Kit by Bar Mills as a starter (see the Mar/Apr 2021 Dispatch). The front of Regent movie theater uses two HO City Classics walls, with scratch-built marquee, ticket booth and entry doors.*



*Bottom: Gaylord's S scale layout depicts the PRR's Northern Division in 1953. He views his layout as a 3-D artform, where the railroad equipment blends with the structures, vehicles, scenery and backdrop to convey a particular place and time.*

Now, please meet some other talented modelers.

## Lynn McCurdy

I'm the former President (we rotate every so many years) and now Media Director of the High Desert Modular Model Railroad Club. The HDMMRC is found at [www.hdmmrc.com](http://www.hdmmrc.com)

About 25 or more years ago, was trying to get my son interested in model trains,

but it soon became obvious that I was more interested than he was. When a friend told me there was a group of model train guys meeting at the hobby store nearby (15 to 20 miles if you consider that near). I had NO idea on modeling, but from the day I entered the store, they pulled me in and started teaching. Now, I could teach them.

I have G to N Gauge/Scale trains. But like the "HO" scale due to size and price. Smaller is hard to work with as you get older, and larger just costs too much.



*My Two Foot Unlucky Mine is smallest at 2 feet.*



*Run Down City is 8 feet.*



*The Monolith cement plant is the largest at 12 feet.*



I'm kind of "all over the place" when it comes to what I do best. But scenery seems to fit my taste and shows the best. If you think Lynn can help you, contact him at [Lynn.McCurdy@sscaleresource.com](mailto:Lynn.McCurdy@sscaleresource.com). Thanks Lynn.

And now please read about Russell Jacobs model railroading that started with a Store Window Display!

## Russell Jacobs



When I was three my grandfather gave me a train set and, as usual, it was a Marx. Somehow at that age things like that are not really expected to last long, but I still have it and it's all still there and runs.

Growing older, at age eight the first Flyer showed up, it was the Pacemaker set. I was in heaven. This all came about after my dad was buying paint and the fellow had a Flyer layout in the store window. My dad sent me over to see it and I met my mentor, Mr. Clark.

It wasn't long after that that Mr. Clark made a gauge change to HO. Being the recipient of most of his Flyer, along with prized buildings, you could hardly drag me out of the basement.

This went on for years, and Mr. Clark would drop in from time to time to check my progress and answer questions if there was a problem. Soon after, Mr. Clark passed and I was on my own. This marked the beginning of the end for this phase as Vietnam loomed on the horizon and this all had to be put away.

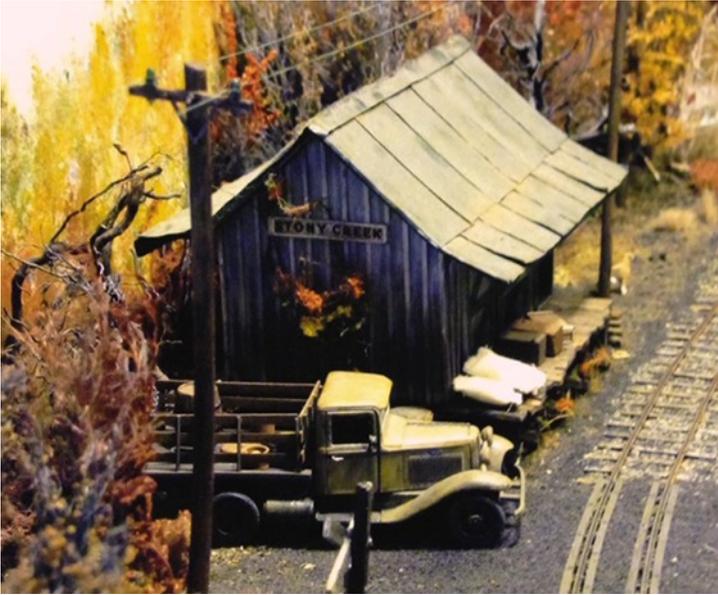
While in the Navy, the bug bit again but on a much smaller scale. N scale. Size in the service is always restrictive and N was the answer.



*Above: This is a scratch built water tower on my boss's Flyer layout.*

*Right: This is a model of the store built just from the picture.*





*This is a scratch built way station on my On30 layout.*



*Sometimes I will go down a different path and do a little pole work.*

Right now, Flyer is back in my system after building a layout for the owner of the auto dealership where I work. The bug bit hard this time and with everything else, space has become a problem again.

As far as building is concerned, it's been trial and error but always learning. The main object today is keeping the hobby alive and to try helping those wanting to learn. Why not, I'm still learning after all these years.

If I can help your modeling contact me at [Russell.Jacobs@sscaleresource.com](mailto:Russell.Jacobs@sscaleresource.com).

Now lets take a flight to Queensland Australia and meet our next modeler.



*Grant Dodd driving the real thing which in this case is a 13,000 ton 126 car long coal train.*

### **Grant Dodd**

I live in regional Queensland which is an east coast state of Australia. I have had an interest in railways since before I could walk, and remember my mother telling me the story that I would want to be pushed in my push-chair down to the local roundhouse in the town that I was born in, in New Zealand, to look at the steam locomotives. My interest in railways started back then, and in one form or another, has remained with me ever since. Modeling for me started at about the age of eight and included both railways and aircraft. I went on to spend many years working as a train driver in both Australia and New Zealand, as well as, having been a train controller (Dispatcher) in New Zealand. Incidentally, I also kept up my interest in aviation and became a pilot.

By the age of 10, I had built my first very basic HO/OO layout in the family garage. By the age of 13, I had advanced to a purposely built hobbies room and was running an assortment of English and European OO/HO stock. A house move by the age of 16 put an end to that layout. By my late teens, there was no more modeling, but by that time, I was using the income gained by being a trainee train driver to finance flying lessons and motorcycles. The early 20's saw a short interest in N gauge, but I found N gauge too small for my liking. Modeling was then out of my immediate interest sphere, but in the back of my head I always thought

that I would build a layout in retirement, and though a few years away at present, I thought that I would make a start well before then. I had run out of projects, and a move to the central Queensland coast took me away from being an active pilot so there was spare finances and time available to direct towards a layout.



*Double headed fast freight climbing the grade after leaving the western end of the tunnel. The siding in the foreground leads to the grain sidings.*

When planning my current layout, I dismissed an English based layout as OO is an accurate enough scale for my liking, and I dismissed European HO as most prototypical European railways are electrified and I wanted to avoid overhead centenary. American outline was also ruled out as the trains are too long to model accurately within the space that I have available, and as the western USA is one of my favorite locations, I didn't have the space to model the grandeur of the landscape to my satisfaction. I wanted to avoid N as I find it's too small for my liking and either O or S would take up more room than I had available, so the choice had to be HO. We don't have large basements as do a lot of American homes, but most homes here have attached double garages, so half the garage space was where the layout was to be housed.



*Class 422 heading a typical Australian fast freight on the down grade towards the eastern portal of the tunnel.*



*One of my favorite locomotives.*



*Freight train headed by a class 45 waiting to depart Wellingong.*

Over the years, I had lost touch of the developments in model railways in terms of technology, the quality and variety of modern RTR locomotives and rolling stock plus the proliferation of manufacturers, so I was pleasantly surprised to discover what is on the market these days. I finally chose to build an HO layout based on the railways of the state of New South Wales in Australia circa late 70's early 80's. Readers may be surprised to learn that Australia has a huge variety of railway systems including several different gauges as each state developed its own system back in the 1800's early 1900's, and these railway systems make great subjects to model. We have a real eclectic mix of locomotive styles, rolling stock and signaling systems, as well as, various gauges. These days, there are private and publicly listed railway companies plus a hand full of state government operators totaling approximately 20 operators.

My layout lives in one side of our garage and is built on four transportable modules so a house move won't catch me out again. Living where I do, there are no clubs or help so I have had to manage by trial and error and a lot of help via the Internet. After five years of work, I have created a layout that I'm happy with and has proven to be reliable to operate while fulfilling my creative desires for the time being.



*The small freight depot at the eastern end of Wellingong. The house in the foreground is an old crossing keepers residence.*



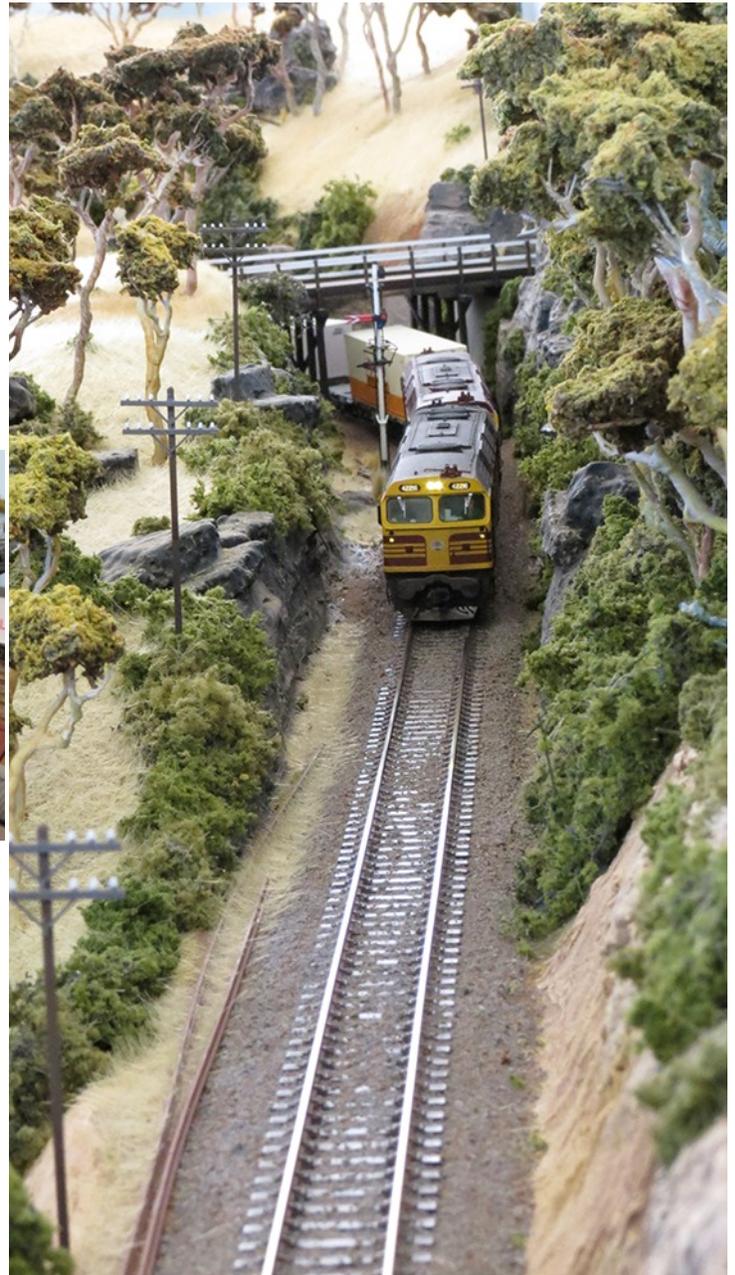
*4509 crossing over the girder bridge.*



*The eastern end of Wellingong showing the freight depot with two class 45's heading a freight train into the station.*



*An overview of the main Wellingong yard with the loco depot in the foreground.*



*Double header class 422's heading a fast freight emerging out from the cutting that leads to the western end of the tunnel.*

I have used an NCE DCC system, but have kept all turnouts and accessories operated by either push buttons or toggle switches. I have kept the electronics simple as I'm no electrical engineer. All rolling stock is RTR, but buildings are a mixture of kits, modified kits and scratch built buildings. Many lineside features are scratch built, including all running signals. At present, the layout provides continuous tail chaser running, but a future planned move will be to a house which we own in another location that provides more space and will allow the layout to be extended by means of a branch line into an adjoining room, subject to wife approval.

All layouts are a compromise, and mine is no exception. The layout is based on a busy single line branch line and features a continuous double loop around the board with a station, a grain siding and two hidden staging sidings which are accessed via two tunnels. The station allows for prototypical shunting (switching) with a freight depot, plus arrival and departure roads with head shunts. There is also a modest loco depot and it lacks a turntable, but as most of the locos are double cabbled and even the hood units have controls facing both short and long hood ends, there is no need for a turntable. I run a mixture of general freight trains which include four wheel wagons, fast freight trains which are bogie wagons only, grain trains and passenger trains. I don't run a card system, but do vary loads for up or down trains to reflect the different traffic that would be carried to and from a branch.

My satisfaction comes from most aspects of the hobby with the exception of wiring. I imagine that I'm like a lot of people and depending on what mood I'm in reflects what I want to do for the day. Watching a slow

freight train trundle around can be just satisfying as weathering a new loco, scratch building a new structure or some random micro detailing. Some people like fishing or gardening to unwind where I find modeling is the great unwinder. Modeling teaches us a multitude of micro engineering skills, but the most valuable skill is patience and you can't learn that on the Internet!

I'm no master modeler, but what I do have a good knowledge of prototypical railway operation, track layout and signaling and interlocking. This real life knowledge is invaluable as I instinctively know where to place a signal or when weathering a loco where on the running board oil will be spilt. The list goes on.

As I have said, I have picked up a lot of skills along the way so I'm happy to share and mentor, and hopefully inspire.

Grant, nice to meet you and thanks for your help. Contact Grant at [Grant.Dodd@sscaleresource.com](mailto:Grant.Dodd@sscaleresource.com).

### Rich Westerman



My first modeling experience was as a youngster building plastic model aircraft and military vehicles. My father was employed by the Illinois Central which allowed me, as an employee's child, to ride free anywhere I wanted on IC passenger trains. Since there was direct IC service between Kankakee, Illinois and Carbondale (where I attended college), I put in countless miles and hours on The City of New Orleans and other IC trains.

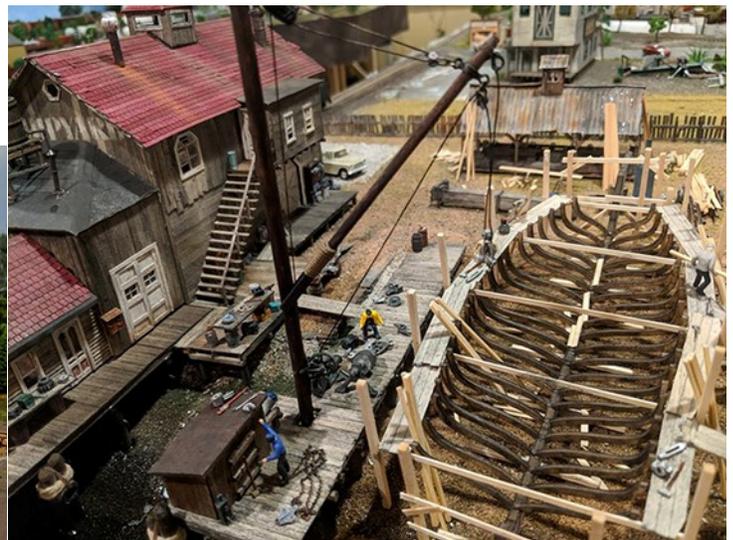
I began my first HO layout at 10 or so with a 4 x 8 sheet of plywood and one of Lionel's first HO train sets. I served in the Navy Submarine service in the late 60's, got married upon leaving the navy and eventually had 4 children and several foster children. I attempted to get my kids interested in model railroading, but it just wasn't for them. so it wasn't until I retired from a career as a social worker that I was finally able to return to the hobby in a serious way.

I've been back in the hobby for 5 years and I'm now on my 4th layout. I didn't intend to build serial layouts, but building each one taught me so much that I had to tear the previous one down and start again using my new found skills and knowledge.

My current layout is a "G" shaped double twisted dog bone. Its a 15' x 22' walk around layout with a single mainline continuous run, connected to a point to



*Scratch built tower with other parts from various kits. windows from Rusty Stumps. Washes and chalks were used.*



*Shipyards from SierraWest kit. Some bashing, but mostly straight from the pictures. Water is 2 part epoxy. River bottom is dirt/sand.*

point class 3 grain hauling shortline. My intent is to replicate the Illinois Central RR in the 70's with central Illinois as the location. My shortline is modeled after a real railroad, the Kankakee, Beaverville, and Southern, which actually interchanges with the IC (now CN) in Kankakee.



*Fosscale pest control kit repurposed as auto body shop. Corrugated roofing of aluminum foil. Washes and chalks.*



*Painted Lady from Branchline Trains Laser-Art Kits. Tinted and detailed with paint pens.*

I have come to realize that while I have a keen interest in trains, my real joy and fascination is in recreating the visual memories that I have of life around the railroad towns of my childhood, which to me means the architecture and rough and ready structures that humans create.

At this point in my modeling, I build only what are referred to as “craftsman wood kits” as well as scratchbuilding and kitbashing to achieve the images that I see in my mind and memory.

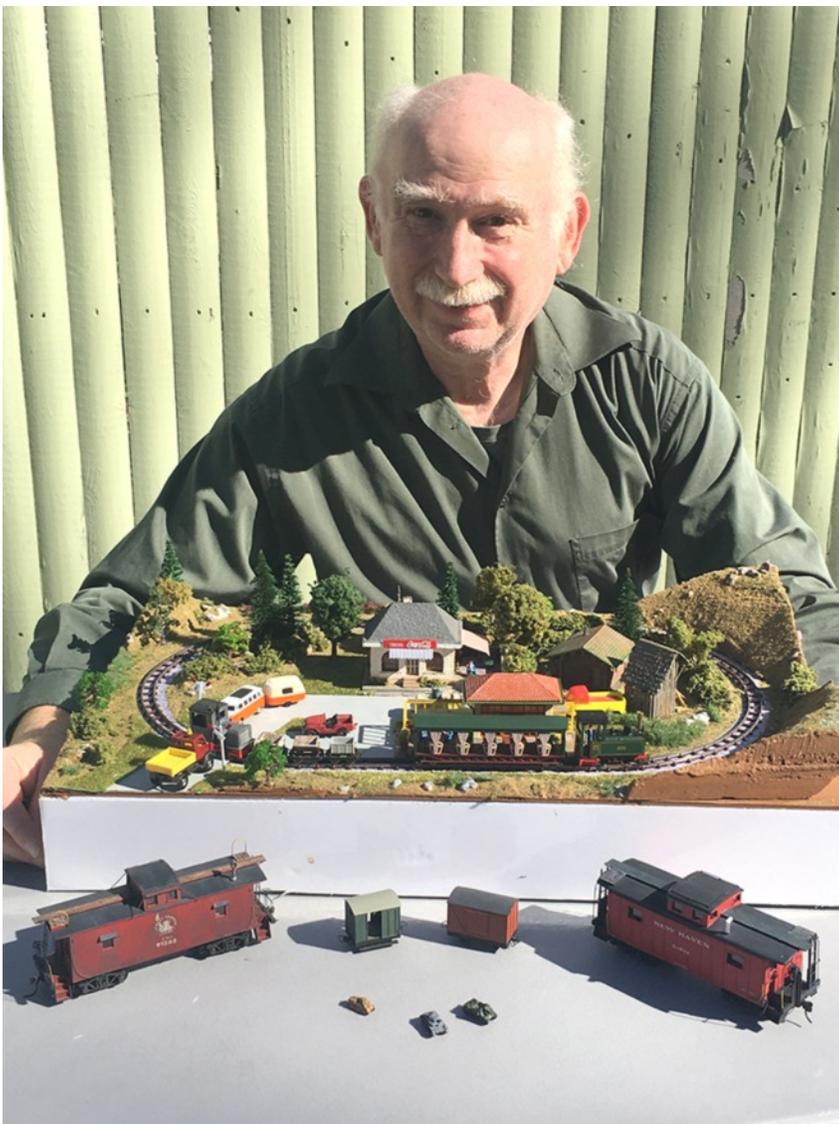
I think that I am a good example of the adage “practice makes perfect”. My work is of course not perfect, but I have learned so many tricks, shortcuts, and particular skills in my pursuit of my hobby that I can serve as proof that if you start out slow and don't beat yourself up over your mistakes, you can achieve many things. The buildings that I built just 5 years ago and was so proud of now make me cringe when I look at them. I take that as a sign that I have made much progress while still having lots of fun with this hobby.

I should mention that I spent a decade in the late 70's and early 80's building guitars and other stringed instruments, where I gained a lot of knowledge and lore concerning the working of wood, as well as a love of wood as a medium of expression. Googling my name will usually reveal a few of the 200 or so instruments that I built during those years. Those instruments are now in the hands of many working musicians literally around the world since I eventually specialized in the development and construction of Irish bouzouki's, which are now a major component of Irish traditional music.

I had no mentors except for helpful FB train group modelers who described and showed their work. Some YouTube videos were helpful, too. I can offer help in most areas of RR modeling. I am strong in structure building/bashing, not so much in electrical.

Please contact Rich if you think he can help with your modeling at [Rich.Westermann@sscaleresource.com](mailto:Rich.Westermann@sscaleresource.com). Thanks Rich for your help and interest.

How about small layouts? Please meet the following modeler.



### **Peter Roos**

This is me with my recent Small Layout in HOn30. Just a little loop with equipment I have had since I was a kid and buildings and scenery material from my stock or given to me.

In front of the layout are two HOn30 cars scratchbuilt years ago, two S scale (my primary RR modeling scale) cabooses, a scratchbuilt CNJ car and an extensively modified American Flyer body turned into a NH NE-5.

The tiny dots in front are 1/285 scale “micro armor” tanks for war gaming. I paint those too. Photo by my wife, Luzella.

My dad started me in modeling, both plastic models and trains. He also started me in S scale, although I took long side trips in HO and HOn30. Small layouts started with the “Elk River Saga” in *RMC* by Dave Frary and Bob Hayden, which also got me interested in Maine 2ft and HOn30. Most skills since have been learned from articles and more recently from Internet videos.



*S scale wood kit buildings assembled and painted.*



Module sceniced for Connecticut S Gaugers based on a painting.



American Model Builders Strong, ME station on the SR&RL, HO laser cut kit with the height of the cupola corrected.

I have skills in building with plastics, kitbashing, painting with brush or airbrush, weathering and figure painting. If I can help you with your modeling contact me at [Peter.Roos@sscaleresource.com](mailto:Peter.Roos@sscaleresource.com). Thanks Peter.

Well it's time for me to get back to my workbench and travel down some more "New Tracks". Please visit my Facebook page [Jim Kellow MMR](#) and follow/like it to stay in touch. Also, please register at my website: [NewTracksModeling.com](http://NewTracksModeling.com) to get email notices for all my Zoom "New Tracks Meetups" every Wednesday and Saturday evening at 7pm Eastern time. We have different talented modelers in each show that can give you some ideas for your own model building. Join us and participate.

Thanks again for reading this far, and best of model building to you.

**Excelle Lubricants distributed by JT's Mega-Steam**

# Excelle Lubricants

- Special Viscosity for S Scale Trains
- Oils are plastic compatible and conductive
- Convenient 4-pack; or purchase individually
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# Freight Car Modifications

## Part IV – A CNW Viking Roof Boxcar

By Tom Lennon

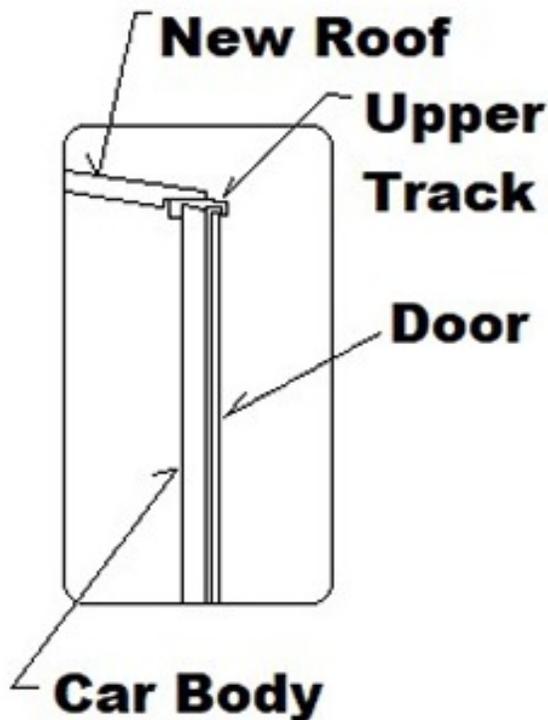
Last time, we modified a couple of SHS USRA boxcars to closely match a particular pair of GN Rwy. prototypes. This month's inspiration came from an ad for an HO car kit.

A while back, Sunshine Models of Salinas, CA offered a resin kit, in HO scale, for a C&NW, RI or Omaha Road USRA rebuilt boxcar. The prototype had steel sides and ends, a steel door, and a Viking roof. I had a ready to run SHS steel rebuilt Xm USRA car (#00126) for a C&NW rebuild, but the SHS car has the standard Murphy XLA style panel roof. Also, a few years ago, Ron Sebastian of [Des Plaines Hobbies](#), tooled up a Viking roof to fit the Pacific Rail 40' boxcar kits. He ran a couple of Midwestern road names in the kits, and had a few extra roof moldings to sell, so I bought one and had it squirreled away in the parts drawer in my shop. I think he still has some available.



The first step in the conversion is to remove the SHS roof and details from the car, without doing too much damage to the rest of the pre-painted and lettered car. I didn't want to have to repaint and decal this one as the quality of SHS's work is too good to wreck. So first, I gently pulled the two grab irons from the corners of the roof walk, and also the "U" shaped support brackets on the ends of the roof walk.

Then I used a razor saw to remove the roof walk from the rest of the roof. It's molded in place and must be sawn away. Take care to not mar the top and sides of the walk, as we'll re-use it later. I used an Exacto knife



to score the plastic where the long walkway meets the corner landing portion, and snapped them both free. They'll be relocated a bit differently when we remount the walkway on the new roof. Once removed, you'll need to file and sand the bottom side of the 3 pieces to thin them as much as possible, without sanding thru the plastic that keeps the walkway boards together.

Then I used a combination of saws, files and sanding sticks to remove the entire SHS roof from the body. Some portions are not well glued to the body and are easy to remove, but others are quite well bonded together. Try to save the portion of the roof molding that contains the upper door guides for later use on the new roof. Also remove the "V" shaped reinforcing strips from the ends that are located right under the roof line. That will leave a slight bit of paint damage, but not so much we can't touch it up later. If you don't remove the strips, the body will be just a bit longer than the roof. Take your time and be careful to not mar the paint and finish of the sides and ends.

As I mentioned, the Viking roof molding is meant to be mounted to the Pacific Rail Shops (now Gold Coast Models) 40' boxcar, and has locating ribs to match that

body casting. Luckily, the pitch angle of the roof on both cars matches up well enough to fit, but the underside of the Viking roof must be carved away at so it will fit into the SHS body. I then took the saved upper door guides and remounted them to the car body. This leaves a slight "hump" along the line of the car body. I carefully filed and sanded this until I reached a point where I thought it might all fall off of the car, and stopped. I then took the Viking roof, and placed it on the car body. I noted the location of the remaining "hump" on the new roof, and again with files and sanding sticks I removed just enough material from the underside of the roof to clear the door track. Glue the new roof to the body and let it all dry. I came back later with some Squadron brand putty, and filled a couple of very small gaps between the roof and body.

There are small tabs along the length of the roof for mounting the roof walk, and they are a bit too tall for the car. I simply put a piece of 100 grit sandpaper on a piece of window glass, and took the car in hand, upside down, and gently sanded the tabs down, almost to where I was sanding roof ribs. You can now glue the long walkway to the tabs. Then glue the corner platforms to the edge of the walkway and the top of the new roof. Now use a small drill bit to drill 3 holes for each of the corner roof grab irons and mount them. Locate and glue the "U" shaped roof walk end braces back where they were.

Whew! Now it's time to paint this bad boy. I tried several mixes of roof brown, oxide red and boxcar red to try and exactly match the factory applied car color. I never did get it exact, but I got close enough. I masked off the factory painted and lettered sides and ends, and sprayed the roof and roof walk. Remove the tape and use a small tip brush to touch up the areas on the ends where there was some damage. I finally used a very light weathering mix over the whole car to blend and disguise the mismatched roof to the rest of the car. It worked OK, and I'm happy with the results. Now someone will write me and tell me that the C&NW had these repainted cars with a black roof, and all the color matching was unnecessary! Oh well.

If you all like this series, we'll continue with some different types of cars we've modified over the years. The list of possibilities is endless. Remember to have fun!

Tommy

# WHAT'S ON YOUR WORKBENCH?

I decided to add guardrails for the Nighthawks scene, and I used available strip styrene. The vertical .08" posts are spaced 1" apart with a scrap aluminum block. The base is .1" strip material, and the top is HO 4x12, which is about a 9" top rail for S gauge. By splicing together the strip material, the total length is about 41". It's flexible and strong.

The back side fabrication picture shows how the 4x12's form an L angle. Evergreen L angle would be better suited for this job, but I was using what I had. - Chris Monje



# S SCALE SHOWS & MEETS

## 2021 NASG Convention

August 3<sup>rd</sup> through 7<sup>th</sup>, 2021

2021 NASG Convention is announced for Buffalo, NY.

The 2021 NASG CanAm Convention will be held in Buffalo NY, August 3<sup>rd</sup> through 7<sup>th</sup> at the Buffalo Marriott Niagara in Amherst. The city of Buffalo has undergone a stunning revival in recent years with its downtown Art Deco architecture, its lakeside setting, and its extensive rail facilities. Mark it on your calendar as a “must-attend”.

[Check their Website here!](#)



## O & S Scale Midwest Show

September 17-19 2021

Indianapolis, Indiana

**New dates announced for this year:**

**October 8<sup>th</sup> through 10<sup>th</sup>, 2021**

**New lower pre-admission for this year:**

**\$20.00**

**New lower hotel rate for this year: \$105.00**

This is a dedicated 2 rail O Scale and S Scale show; however, we encourage and welcome the many modelers and collectors from the 3 rail and high rail side of the hobby to attend. There are many aspects of the hobby, including building, scenery and more that applies to any scale. Moreover, this show is a great place to get inspired while meeting old friends and making new ones!

Website: [scalemidwest.com/](http://scalemidwest.com/)

## S-FEST 2021

An Annual Midwest S-gauge & S-scale Event

November 5<sup>th</sup> through November 7<sup>th</sup>, 2021

Four Points by Sheraton

5311 S. Howell

Milwaukee WI

Hosted by Badgerland-S-Gaugers

Website: [trainweb.org/bsg/events/sfest.htm](http://trainweb.org/bsg/events/sfest.htm)



## O Scale, S Scale, Narrow Gauge West

May 27-29, 2022

Hyatt Regency, Santa Clara

5101 Great America Parkway

Santa Clara, California

O Scale – S Scale – Narrow Gauge West is the largest 2-rail O scale convention west of the Mississippi, and the largest S scale convention west of the Mississippi.

Email: [info@oscalewest.com](mailto:info@oscalewest.com)

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